



# AGENDA

## DESIGN REVIEW COMMITTEE

May 3, 2018

5:30 p.m.

2<sup>nd</sup> Floor Council Chambers

1095 Duane Street • Astoria OR 97103

1. CALL TO ORDER
2. ROLL CALL
4. MINUTES
  - a. August 3, 2017
3. PUBLIC HEARINGS
  - a. Design Review DR17-03 by Garry Vallaster, Astor Venture, LLC to construct an approximately 11,580 square foot commercial building at 2350 Marine Drive (Map T8N-R9W Section 9CB; portion of Tax Lot 6803; portion of Block 144, Shively's, within the Gateway and Civic Greenway Overlay Zone in the LS (Local Services) Zone
1. STATUS REPORTS
1. REPORT OF OFFICERS
1. PUBLIC COMMENTS (Non-Agenda Items)
- ADJOURNMENT

THIS MEETING IS ACCESSIBLE TO THE DISABLED. AN INTERPRETER FOR THE HEARING IMPAIRED MAY BE REQUESTED UNDER THE TERMS OF ORS 192.630 BY CONTACTING THE COMMUNITY DEVELOPMENT DEPARTMENT, 503-338-5183.

## DESIGN REVIEW COMMITTEE

Astoria City Hall

August 3, 2017

### CALL TO ORDER:

Vice President Gunderson called the meeting to order at 5:35

The Commission proceeded to Item 3: Roll Call at this time.

### ELECTION OF OFFICERS – ITEM 2:

This item was addressed immediately following Item 4: Approval of Minutes.

In accordance with Sections 1.110 and 1.115 of the Astoria Development Code, the Design Review Committee needs to elect a new Secretary for 2017. The previous Secretary was Sherri Williams. It is recommended that Anna Stamper be elected as Secretary for 2017.

Vice President Gunderson moved to elect Anna Stamper as Secretary for 2017; seconded by Commissioner Phelps. Motion passed unanimously.

The Commission proceeded to Item 5: Public Hearings at this time.

### ROLL CALL – ITEM 3:

This item was addressed immediately after Item 1: Call to Order.

Commissioners Present: Vice President LJ Gunderson, Leanne Hensley, and Hilarie Phelps.

Commissioners Excused: Jared Rickenbach and Derith Andrew.

Staff Present: Planner Nancy Ferber. The meeting is recorded and will be transcribed by ABC Transcription Services, Inc.

### APPROVAL OF MINUTES – ITEM 4:

Vice President Gunderson called for approval of the minutes of the January 5, 2017 meeting. Commissioner Hensley moved to approve the January 5, 2017 minutes as presented; seconded by Commissioner Phelps. Motion passed unanimously.

The Commission proceeded to Item 2: Election of Officers at this time.

### PUBLIC HEARINGS:

This item was addressed immediately following Item 2: Election of Officers.

Vice President Gunderson explained the procedures governing the conduct of public hearings to the audience and advised that the substantive review criteria were available from Staff.

### ITEM 5(a):

DR17-01 Design Review DR17-01 by Mike Stults, Cross Development, to construct a new 9,100 square foot commercial building at 2275 Commercial within the Civic Gateway and Greenway Overlay Zones in the LS, Local Service Zone.

Vice President Gunderson asked if anyone objected to the jurisdiction of the Design Review Committee to hear this matter at this time. There were no objections. She asked if any member of the Design Review Committee had any conflicts of interest or ex parte contacts to declare. Vice President Gunderson declared that she went online to look at the history of the business to see what their storefronts looked like over the years. She called for a presentation of the Staff report.

Planner Ferber reviewed the Findings and Conditions contained in the Staff report. She noted the public notice contained an error on the tax lots, but the address was correct. Staff resent the notice with the correct tax lots. No correspondence prior to the meeting had been received and Staff recommended denial of the request.

Vice President Gunderson opened the public hearing and called for testimony from the Applicant.

Mike Stults, 4336 Marsh Ridge Road, Carrollton, TX, stated Cross Development represents the tenant, Dollar General. He understood the tenant was not critical to the review, but often times they are the elephant in the room. No matter who the applicant is, there is always a bias for or against the tenant. Usually, there is some misunderstanding of the client he represents. He and his tenant want to be good members of the community. He understood that their interpretation of the ordinances was not necessarily going to be everyone else's interpretation. He was present to gather information, seek the Commission's graces, and follow through with whatever that might be. He gave a PowerPoint presentation titled *DG at a Glance*, which provided general information about Dollar General Stores. He confirmed that he understood some of the information provided in his presentation could not be reviewed by the Commission. However, the public is usually very concerned about what is coming to Astoria. Several Dollar General Stores have been approved in Oregon and they sign 15-year triple net leases. Dollar General has core values, missions, and a prominent literacy foundation program that support local libraries. The stores are diminutive in size compared to a large grocery store or super center and they carry typical brands. The aisles are kept tidy. He and the architect went through the design guidelines in Chapter 14 and they made their best effort to capture elements of local designs and incorporate them into the proposal. He believed he would have a tough time accommodating the massing requirements. Typical downtown shopping areas have buildings right on the street with a large community gathering area right on the street. There is often a remote parking field and people will walk a block to get to the shops and restaurants. This store is more of a general mercantile with parking on site. The location is excellent for a Dollar General because it serves the community in a great capacity. The orientation and site lay out offers one location for the building. He can install brick pavers, park benches, and move the entrance to the northwest side of the site. He believed moving the entrance would not provide good service to the most visible feature of the building. The building will face a large curb cut. Pedestrians will be able to see where the entrance is located and safely congregate on the public sidewalks. It would be impossible to relocate the building on this site because doing so would lose parking and the ability for trucks to make deliveries. He can paint the building any color and add any elements the Commission would like. But in order to come to a conclusion that this is a supported project, the safety of getting cars in, receiving deliveries, and the ability to match the building to the site are the big issues that need to be addressed. The Applicants have made their best effort and they would continue to dress up the sidewalk, add benches, do some special landscaping, and create more flair in the pedestrian area. However, it will be difficult to put the building in a location that would not allow for any parking or deliveries. He was happy to continue submitting to Staff and do whatever is necessary to work with the City.

Commissioner Phelps understood the Applicants believed the massing could not be changed. She confirmed that they would be willing and able to work with Staff on the other issues addressed in the Staff report.

Mr. Stults added that the spandrel glass proposed for the windows could be eliminated to allow visibility inside the building. He confirmed that he had submitted several rounds of changes to the original proposal prior to this hearing. The application process was a surprise because he believed they had done exactly what Chapter 14 itemized. They found out their proposal was not what Staff was looking for, so they visited many other sites and buildings in Astoria before reintroducing what they believed were design elements from the community. Now, they understand that was not what Staff was looking either. They still feel like the design criteria in Chapter 14 can be met, except for the position of the building. However, if their proposal is not the vision of City Staff, they can accommodate. There is a four-story medical office plaza right beside the lot that has barrel roofing. Only two buildings in town have a similar style roofing, but the rest of the commercial buildings in Astoria have rooflines like the one they have proposed. He believed the roof would look at home. It would be a significant challenge for Dollar General to create a large barrel roof and the City encourages roofs like the one proposed.

Vice President Gunderson asked Mr. Stults to show here where the entrances would be located.

Mr. Stults showed the location of the windows and the entrance on the screen, noting that the entrance would face the center of the line of site from the street. Drivers and pedestrians would be keenly aware of the entrance to the building. The building would not fit if it were located on the lot as suggested by the City. The angles of the

lot force the building to be located as proposed. They could put the entrance to the building in another location, but that location would not be where people will walk or drive up to the building.

Commissioner Hensley said she was not in favor of the proposed siding because it seemed outdated and looked too residential. She preferred a siding material that was more aesthetic and congruent to the rest of the community. She asked how long the Applicant had been working with Staff on changing the original proposal.

Mr. Stults said he had been working with Staff for two months and had proposed a different material at first. He believed the siding was modern, but not as modern as what is in Portland. So, this proposal was their second attempt at an appropriate siding material. What looks good in Bend or Portland is not Astoria, so he drove around town to find the common threads so they could represent something local. The proposed siding can be seen all up and down Astoria and he was advised to refrain from new trends.

Commissioner Hensley stated she understood Mr. Stults' reasoning, but still agreed with Planner Ferber that a different material would be appropriate in that area. The details are important and if you do not have the right selection initially, you have to keep refining and getting creative.

Mr. Stults noted that the medial pavilion has lap siding with corrugated steel. His original proposal was for lap siding with panel steel.

Commissioner Hensley said she believed signage details should be submitted because signage is a prolific component of a commercial building, especially if branding is an element. The gooseneck lighting was nice, but the Applicant should continue to implement Staff's recommendations. She asked for more details about the proposed awnings and wall treatments.

Mr. Stults stated he reviewed the awning and wall treatment requirements in Chapter 14 and visited Astoria to look at what had actually been installed on buildings. That is what he proposed. It is his understanding that the Code requirements and what can be seen on the streets is not what Staff wants to see. He is not asking that Staff design the building for them, but they have made their best guess at what they believe was being suggest by the Code, on the street, and by Staff. They will continue to respond.

Commissioner Hensley said she believed the Applicants could follow through more with some of the recommendations.

Mr. Stults said he received Staff's recommendations last week and are now happy to respond again.

Commissioner Hensley added that because the site plan was so unique, she believed the Applicants should continue to follow up with Staff on landscaping.

Vice President Gunderson said she was concerned because the packet was full of things that had not been presented.

Commissioner Hensley stated the outside aesthetics of the building should be married with the whole concept. The typical Dollar General stores shown did not match the exterior schematics proposed.

Mr. Stults noted the exterior schematics were being updated and he would be happy to give them to Staff.

Vice President Gunderson called for testimony in favor of the application. There were none. Vice President Gunderson called for testimony impartial to the application.

Russell Thompson, 265 23<sup>rd</sup> Street, Astoria, said his major concern was the lighting. He lives 75 feet from the proposed corner of the building and the light shining all night should be directed away from Mill Pond. Public Works will have to do work on the corner of 23<sup>rd</sup> and Commercial, which is two houses down from his house. He was concerned that a traffic signal will be installed at that intersection when the City needs a traffic signal closer to the main street.

Jeff Newenhoff, 1563 Irving, Astoria, said he owns City Lumber, which is directly across the street from this project. He was excited about having more customers coming to the area, but was concerned about traffic and



parking. In 2008, he had considered tearing down his store and building a new one. The Transportation Department wanted him to change the intersection from a Y shape to a T shape, which would have created a no parking zone all the way across his property. He was concerned that this project would result in the same situation. No parking in front of his store would severely impact his business. He planned to make a presentation to the Commission next month showing plans to develop his property and hoped the Commission could find a way to address the traffic concerns. Additionally, Public Works is supposed to address concerns at 21<sup>st</sup> and Commercial, not 23<sup>rd</sup> and Commercial. He handed a letter to Planner Ferber.

Vice President Gunderson called for testimony opposed to the application.

McLaren Innes, 4807 Birch Street, Astoria, said she was on the Planning Commission for several years and understood the orientation she should offer this Commission. The overall Comprehensive Plan objectives do not figure into what she sees presented as a plan by the Applicant. There will be no enhancement of primary uses nor compliment to the downtown area. It is obscene how different the proposal reads as part of the Comprehensive Plan, which is tested to a maximum all the time. This project does not meet most of what is defined in the Staff report. The Commission and Staff have done a wonderful job addressing the items, but she was pleased with the town and its way of growth and this does not fit. She would appreciate denial of any permits for the store's existence.

Doug Thompson, 342 14<sup>th</sup> Street, Apt. 602, Astoria, said he was on the board of the Lower Columbia Preservation Society (LCPS), but was speaking tonight as a private citizen. He noted that this matter had not been considered by LCPS and the project was not adjacent to or near historic structures. When he served on City Council, he had proposed the first master planning effort in Astoria, called the Gateway Master Plan. The plan contained the first design review process that led to this Commission. He believed the design review process was very good and the Commission has been presented with a first class Staff report. The purpose of a Staff report is to set out the standards that the community wants in order to attract businesses and uses for the community. While this process may seem challenging to developers, it is part of what the City hopes is an ongoing conversation. It took almost two years to develop the master plan and design review process. Dollar General will have a 15-year triple net lease, which is a long-term lease in the current market. But this is the blink of an eye in the contest of the lifespan of a building. The building permitted for that site is likely to remain for at least a century, so this matter is much bigger than Dollar General is. It is likely that the uses of the building will change a number of times during its lifetime. The massing of the structure on that site is the biggest issue and more important than any other element of the project. Current trends that seem like science fiction, like autonomous vehicles and walk-able cities, could lead to doing away with dedicated off-street parking for retail. The Commission needs to think about the long-term horizon. Developers do not want to dedicate most of the square footage of the site to single occupancy vehicles. He supported the entire Staff report and believed the Commission should deny the application and ask the Applicants to work with Planner Ferber. He understood the Applicants were in the early stages of the process, so had not engaged with the Oregon Department of Transportation yet. However, the existing curb cut is grandfathered in and he believed it should be removed. The main entrance and face of the building should be on Commercial, not Marine. This community is overrun with Portland hipsters and their designer dogs on most weekends. Most of them drive down Highway 30 and through that curve, which is not the pedestrian face of the site or the building. The roof should have solar panels. This might not make sense today or five years from now. However, there is some indication that the planet is getting hotter and the days are getting sunnier.

Vice President Gunderson called for the Applicants rebuttal.

Mr. Stults stated the Dollar General would be required to have full cut offs for the lighting and would not be allowed to shine any lights over their property lines. Lights would not be on before or after operating hours. There might be interior emergency lights that could be seen from the outside, but they would not bleed over property lines. If this ever became a problem, it would be rectified immediately. Twenty-eight parking stalls have been proposed. At peak operating hours, across all Dollar Generals in existence, the average parked cars are 12 parking spaces. With half of the parking stalls filled at peak hours, there would be no reason for someone to park in the street. Dollar General is not a local independent business and they will strive to meet all of the criteria in the Comprehensive Plan. He understood they had homework to do and the Applicants were willing to do it. They will invest millions of dollars to clean up the property and turn it into part of the neighborhood.

Vice President Gunderson called for closing remarks from Staff.

Planner Ferber said most of her concerns had been addressed and she appreciated the Applicant's willingness to go back and forth with Staff on so many issues. Her major concern, which had not been addressed, was the massing and orientation. The Code states the project should be a visually continuous pedestrian oriented street front with no vehicle use between the building faces and the street. This is why she suggested moving the building toward Marine or reorienting it. She had also suggested a flat iron style triangle building, but understood this would be expensive. The lot is unusual and it is in a tricky spot, but the massing and orientation has not been addressed. City Codes and Development Codes prevent parking lot lighting from glaring into the neighborhood. Lighting would be reviewed as part of the building permit application. The transportation issues would also be reviewed during the building permitting process and Oregon Department of Transportation (ODOT) is already aware of this project. The barrel roofing was never required, just suggested. She believed the Applicant made a fair point about walking around town to get a lay of the land and the barrel roofing is unique to just a few sites in Astoria. The Staff report suggested a gabled or pitched roof that would blend in to the residential area. The proposed roof is too low and she was concerned about the awning materials. She had suggested lumber, but the awnings ended up looking like a south west saloon. She was also concerned about the bronze detailing on the wall, which looks decorative, but needs to be pedestrian friendly and accessible. She believed brick pavers in the parking lot would be great, especially if the lot becomes bike parking or smart car parking in the future. She would be happy to review additional landscaping plans.

Commissioner Phelps asked why the pedestrian friendly side of the building had to be on Marine instead of Commercial.

Planner Ferber stated the entire site needs to be pedestrian friendly because it is in the Gateway Zone.

Commissioner Phelps stated she liked the flat iron building design.

Vice President Gunderson closed the public hearing and called for Committee discussion and deliberation.

Vice President Gunderson said the Staff report was excellent. She believed a lot of information was missing from the Applicant. Columbia Bank submitted an application to the Historic Landmarks Commission (HLC) for a building design that did not compliment downtown. The Bank was not willing to work with the City. The HLC denied the application, as did City Council. Even though the Applicant has stated they would be willing to make changes to this project, she questioned whether they could do enough to make the Dollar General fit this community. Everything presented looks like a retail box with no character. The proposed building would fit well in Warrenton, not in Astoria. She was very concerned about this building being proposed at the front door of Mill Pond.

Commissioner Phelps agreed the Staff report was very good. The design looks more south west. She wondered why the Applicants proceeded with the meeting after seeing Staff's recommendations. She wanted to see more work towards following the guidelines.

Commissioner Hensley believed the Commission received good feedback from the community and the developer. Many of Staff's recommendations are missing and she did not understand why they had not been implemented by the Applicant after working with Staff for two months. The Applicant has said they would be willing to do many things and has stated their interpretation of the Code. However, it is important for the Applicant to work with the City's interpretation of its own Code. She believed the Applicant should consider the solar idea because it is hot and the climate is changing. Brick pavers would be a great idea as well. There is a lot missing, so she recommended denial of the application so the developer could submit something more creative.

Commissioner Hensley moved the Astoria Design Review Committee adopt the Findings and Conclusions stated in the Staff report and deny Design Review DR17-01 by Mike Stults; seconded by Commissioner Phelps. Motion passed unanimously. Ayes: Vice President Gunderson, Commissioners Phelps, and Hensley. Nays: None.

Vice President Gunderson read the rules of appeal into the record.

#### STATUS REPORTS – ITEM 6:

Staff updated the Commission on the following:

- Columbia Memorial Hospital Cancer Clinic
- Mill Pond Homeowner's Association Annual Meeting

REPORTS OF OFFICERS/COMMISSIONERS – ITEM 7:

There were none.

PUBLIC COMMENTS – ITEM 8:

There were none.

ADJOURNMENT:

There being no further business, the meeting was adjourned at 6:43 pm.

APPROVED:

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Community Development Director

## STAFF REPORT AND FINDINGS OF FACT

April 26, 2018

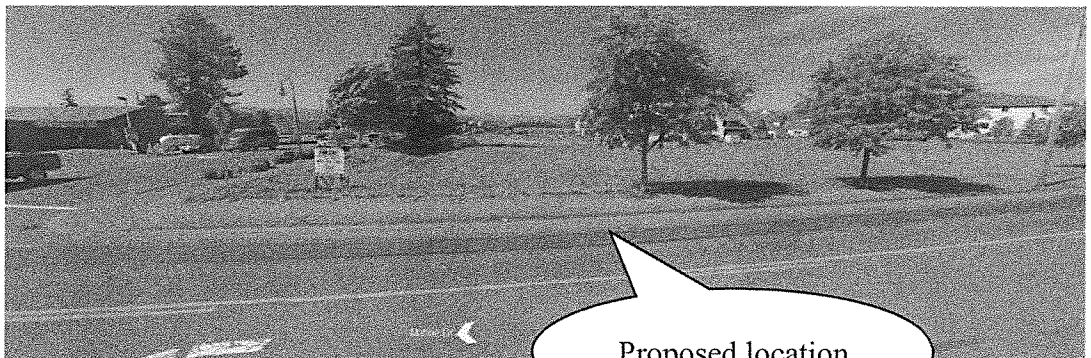
TO: DESIGN REVIEW COMMITTEE

FROM: NANCY FERBER, PLANNER

SUBJECT: DESIGN REVIEW REQUEST (DR17-03) BY ASTOR VENTURE LLC (GARRY DON VALLASTER) TO CONSTRUCT AN 11,580 SQUARE FOOT ONE-STORY COMMERCIAL BUILDING (ASTORIA CO-OP GROCERY) AT 2275 COMMERCIAL STREET

### I. BACKGROUND SUMMARY

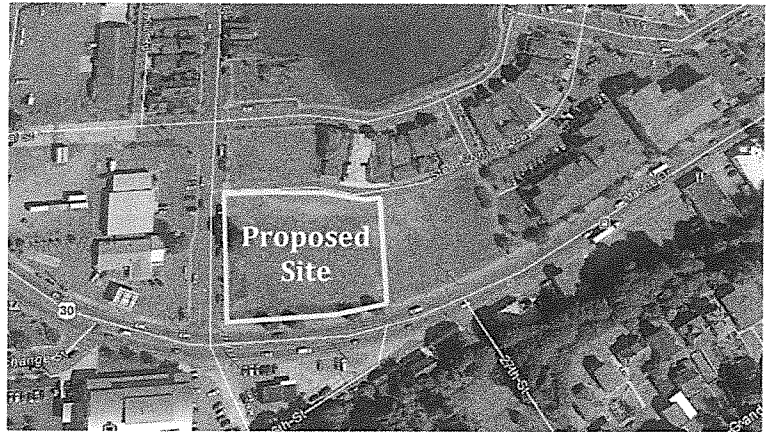
- A. Applicant: Astor Venture, LLC, Garry Vallaster  
PO Box 3933  
Portland, OR 97208
- B. Owner: Astor Venture, LLC, Agent Garry Don Vallaster  
711 SW Alder St  
Portland, OR 97205
- C. Location: 2350 Marine Drive (Map T8N-R9W Section 9CB; portion of Tax Lot 6803; portion of Block 144, Shively's
- D. Zone: LS (Local Services), Gateway and Civic Greenway overlay zones
- E. Proposal: To construct a one-story appx 11,580 square foot commercial building
- F. Previous Applications The site was planned for the Wauna Credit Union (1999-2000) and the Clatsop Care facility (2010-2012), neither of which began construction.



## II. BACKGROUND

### Site:

The property was part of the Mill Pond redevelopment project, initiated by the City of Astoria and Venerable Properties beginning in 1997/1998. The majority of the area is designated Attached Housing-Mill Pond (AH-MP). Development within the zone has been exclusively residential to date, with single family dwellings surrounding the mill pond and affordable



multifamily apartments situated along Marine Drive. There have been several attempts in the past to develop this lot for non-residential uses, including a credit union and a medical facility. The AH-MP zone permits retail uses as a conditional use, but not in excess of 6,000 square feet. In discussions with the applicant, staff determined that it would be more appropriate to extend the Local Service (LS) zone to the east, where retail sales are permitted outright, and to add some restrictions regulating the size. The zone change in the area to allow retail sales over 6,000 square feet at the site was approved and adopted by City Council at the March 5, 2018 Council meeting.

The site lies in the “Gateway” neighborhood in the comprehensive plan CP.057-Gateway Overlay Area. The site also lies within two design overlay zones:

*The Civic Greenway Overlay zone is intended to “protect views of and access to the Columbia River, provide for an enhanced open space and landscaping, support water-dependent uses consistent with Astoria’s working waterfront, and encourage modest scale housing in areas recommended for residential use. It extends from approximately 16th street to 41st Street between Marine Drive and the River.*

*The Gateway Overlay Zone is “intended to be an intensively developed, mixed-use area which complements Downtown Astoria and the community as a whole.”*

The area is dominated by major institutional uses, including the Columbia River Maritime Museum, the City of Astoria Aquatics Center, Columbia Memorial Hospital, the Oregon State University Seafood Lab, East End Mooring Basin, and associated Port property. There are several significant vacant land and water areas suitable for redevelopment.

Area:

The proposed location is bounded on the north by Steam Whistle Way, to east by undeveloped property zoned Attached Housing-Mill Pond (AH-MP), to the south by Marine Drive and to the west by 23<sup>rd</sup> Avenue. The area includes multiple tax lots and multiple platted lots. Prior to any construction, the applicant shall submit a lot line adjustment permit to the Community Development Department to combine the lots and provide legal description and/or a survey of the site.



#### Proposed Construction: At a Glance

This proposal is to construct a 11,580 square foot commercial space to house the Astoria Co-op Grocery. The proposed new building is on a site area of 76,959 square feet. 11,580 (15% of the site) is proposed for building footprint, with an additional area for a loading bay, and a landscaped area of approximately 38,396 square feet (meeting the 20% LS zone requirement for landscaping), a parking lot, storm water drainage bioswales and outdoor seating area.

The proposed use of the site is not under review by the DRC. Retail Sales Establishments of this size, are an outright permitted use in this lot in the recently approved LS-Local Service zone. The applicable criteria, including design aesthetics and orientation of the building are reviewed in this staff report.

Multiple drafts of plans for this project have been submitted to and reviewed by staff. The final set of proposed plans are included with the application materials, and are dated 4-24-18. Previous iterations are referenced in some of the public comments. A full set color plans are available for review in the Community Development Department, and posted on the City's website as a PDF. The plans were too large to attach with the findings of fact for mailings.

**Style:** Single story rectangular shaped building with a flat roof, sloped for drainage. The proposed structure has prominent canopy/awning feature on the east and south facing elevations

**Roof:** The LS zone allows heights up to 45', however the overlay zone maximum is 28'. The proposed roof plan is 20', with a wooden canopy with sheet metal at

25'5". Materials include a white or gray Thermoplastic polyolefin (TPO), and heavy wooden brackets to support the canopy

Siding: The exterior façade is a combination of 8" horizontal stained ship lap cedar siding, a concrete stem wall 4'6" high, and metal wall panels comprised of box corrugation, galvanized and painted zinc gray.

Windows: East elevation will have an aluminum storefront window system, and fiberglass windows, vinyl windows with a white finish are proposed on the other elevations.

Doors: A metal roll-up loading door in zinc gray is proposed on the north elevation. The applicant notes other doors will be hollow metal or fiberglass. Details on the automatic sliding double door have not been submitted. Additional detailing on proposed doors shall be submitted by the applicant.

Other Design Elements: Additional decorative elements include large wooden column and beams with a natural finish proposed on the south and east elevations. Steel metal bike racks are proposed on the east side, and 3 options for screening/fencing around the loading area on the north side of the building.

Exterior Lighting: Two options for parking lot lighting are proposed on page 36 of the proposal dated 4-24-18. Downcast exterior wall mounted lighting proposal is included on page 37 of the proposal dated 4-24-18.

Sign: The proposed development includes aluminum wall signage. Dimensions and materials shall be submitted with the sign permit including clarification on illumination and location and dimensions for any other proposed signage such as freestanding or monument signs. 150sq feet of signage is allowed per the zoning.

Trash Enclosure: Screening or fencing trash enclosures is required, no details were submitted. A site plan shall be submitted prior to obtaining a building permit application for review.

### III. PUBLIC REVIEW AND COMMENT

A public notice was mailed to all property owners within 250 feet pursuant to Section 9.020 on April 10, 2018. A notice of public hearing was published in the *Daily Astorian* on April 27, 2018. On site notice required in Article 9 was posted April 23, 2018. Comments from the following parties were received in time to be included with meeting packets:

- Email from Barbara Bower 3-4-18
- Email and letter from Barbara Bower dated 4-24-18

- Letter from Mill Pond residents dated 4-25-18, with exhibit materials
- Letter from John Ryan dated 4-26-18

Any additional comments received will be made available at the Design Review Committee (DRC) meeting.

#### IV. APPLICABLE REVIEW CRITERIA AND FINDINGS OF FACT

- A. Section 14.015(A) states that in addition to conformance with the specific uses and standards of the individual zones, the following zones shall conform to the general regulations of the Gateway Overlay Zone in Sections 14.005 through 14.030. 1) Maritime Heritage; 2) Family Activities; 3) Attached Housing-Health Care; 4) Health Care; 5) Education/Research/ Health Care Campus; 6) Hospitality/Recreation; 7) Local Service; and 8) Attached Housing-Mill Pond.

Finding: The site of the proposed grocery store would be located in the Local Service (LS) and shall conform to Sections 14.005 through 14.030 of the Development Code. The location is also in the Civic Greenway zone, article 14.035 notes 14.040 B *“Non-residential and mixed use development applications shall be reviewed through the public design review process subject to Design Review Guidelines in Section 14.025.”* This criteria is met.

- B. Section 14.015(B) requires that each public or private development proposal within the Gateway Overlay Zone will be reviewed for consistency with the Design Review Guidelines in Sections 14.020 through 14.030.

Finding: The proposal is a private development to be constructed within the Gateway and Civic Greenway Overlay Zones and as such will be reviewed for consistency with the Design Review Guidelines. This criteria is met.

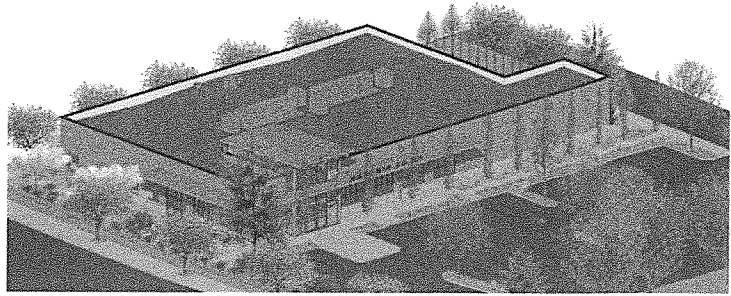
- C. Section 14.020 states that the Design Review Guidelines shall apply to all new construction or major renovation. The guidelines are intended to provide fundamental principles that will assist in the review of the proposed development. The principles identify both “encouraged” and “discouraged” architectural elements. They are broad design objectives and are not to be construed as prescriptive standards.

Finding: The structure is new construction and as such is subject to the Design Review Guidelines.

- D. Section 14.025(A) describes the purpose of the Design Review Guidelines and states that the Gateway Plan *“encourages new construction to reflect building types found in the Uppertown area.”* Three historic building types commonly found in the area include waterfront industrial, commercial, and residential.



Finding: The proposed development is a commercial structure, and at a single story is low in form, which is an encouraged building type. See rendering to the right. The new building is simple with a general rectangular plan shape. It is neither



complex nor sprawling. The canopy adds an architectural element that makes the design unique, while maintaining an encouraged form. This criteria is met.

E. Section 14.025(B) identifies the building forms encouraged.

1. All Building Types: a) Simple designs without extraneous details; b) Rectangular in plan; c) Square in plan.
2. Waterfront Industrial: a) Low in form; b) Cubic in form.
3. Commercial: a) Low in form.
4. Residential: a) Vertical in form; b) Cubic in form; c) Full front porch or front porch large enough to accommodate several seated persons.

Section 14.025(C) identifies the building forms discouraged.

1. All Building Types: a) Complex building footprints; b) Sprawling structures.

Finding: The building would be rectangular with parking adjacent to the front (east) elevation. The building footprint is not complex, nor sprawling. This guideline is met.

F. Section 14.025(D) identifies the windows encouraged.

1. All Building Types: a) True-divided, multiple-light windows; b) Single-light windows; c) Applied muntins with profile facing window exterior; d) Rectangular windows with vertical proportions; e) Fixed windows; f) Double or single-hung windows; g) Casement windows; h) Windows should be spaced and sized so that wall area is not exceeded by window area, with the exception of commercial storefronts.

2. Waterfront Industrial: a) Square or rectangular windows with multiple lights.
3. Commercial: a) Storefronts: 1) Plate glass windows with multiple-light transom windows above; 2) Recessed entries; 3) Window to wall surface proportions may be exceeded; b) Upper Stories: 1) Window area should not exceed wall area.
4. Residential a) Vertical rectangle or square windows; b) Combination of single and multiple-light windows; c) Single windows, paired windows, or windows grouped in threes; d) Bay windows; e) Arched or decorative shaped windows used sparingly; f) Windows should use casings and crown moldings.

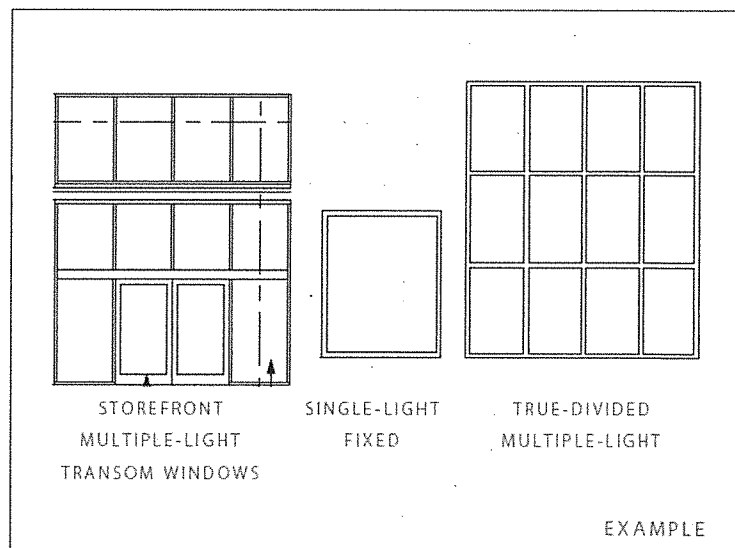
Section 14.025(E) identifies windows discouraged.

1. All Building Types: a) Applied muntins which have no profile; b) Smoked glass; c) Mirrored glass; d) Horizontal sliding windows; e) Walls predominated by large expanses of glass, except in commercial storefronts; f) Windowless walls. Large expanses of blank walls should only be located in areas which are not visible to the public; g) Aluminum frame windows, except in commercial storefronts.

Finding: Proposed windows include both aluminum and fiberglass materials. Large window area is allowable for commercial sites. However, spandrel and frost glass is discouraged. The applicant shall confirm that proposed windows have clear glass. Large blank windowless walls are discouraged, and frosted/tinted windows do not provide the same open transparent feeling.

Additional details on the fiberglass windows on the west wall shall be submitted to confirm if they are single fixed windows or true divided.

Aluminum



window designs have been approved and installed at the CMH Pavilion building and are appropriate for commercial building types.

The applicant has not included enough design details about the proposed metal man doors or double front door to evaluate the design for the doors. Additional details on the proposed fiberglass windows are required to confirm they meet the guidelines.

G. Section 14.025(F) identifies exterior wall treatments encouraged.

1. All Building Types: a) Drop siding; b) Weatherboard siding; c) Horizontal siding with six inches or less exposure.
2. Waterfront Industrial: a) Board and batten style; b) Galvanized corrugated metal.
3. Commercial: a) Finished concrete; b) Brick veneer.
4. Residential: a) Clapboard; b) Wood shingle (rectangular); c) Decorative wood shingle.

Section 14.025(G) identifies exterior wall treatments discouraged.

1. All Building Types: a) Exposed textured, concrete block; b) Flagstone or other applied stone products; c) Precast concrete or decorative concrete panels; d) Wood shakes; e) Plywood paneling.

Finding: The structure is proposed to be sided with a mixture of sidings. Siding examples are available in on page 18 of the proposal. The proposed painted metal siding with vertical ridges, and wood ship lap siding meet the guidelines for materials. The applicant shall confirm the reveal of the siding. Finished concrete is also an encouraged element. The materials meet the guidelines.

Materials proposed for screening the loading bay shall be reviewed by DRC; three landscaping/fencing alternatives have been provided by the applicant for review.

H. Section 14.025(H) identifies the roof elements encouraged.

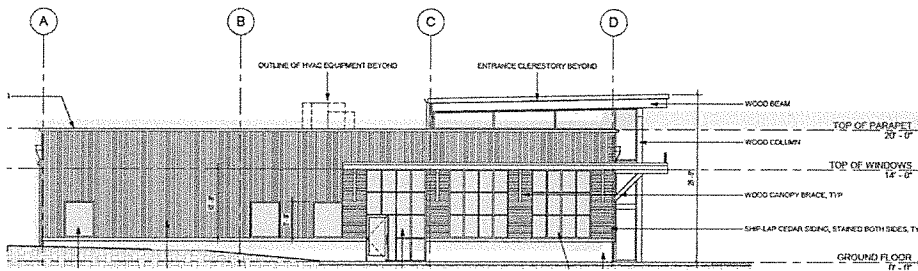
1. Waterfront Industrial: a) Single gable with low pitch; b) Repetitive gable with steep pitch; c) Shallow eaves; d) Small shed roof dormers; e) Monitor roof on ridge line; f) Flat panel skylights or roof window.

2. Commercial: a) Single gable with low pitch; b) Repetitive gable with steep pitch; c) Shallow eaves behind parapet wall; d) Flat or gable roof behind parapet wall; e) Structural skylights.
3. Residential: a) Steep gable with broad eaves; b) Steep hip with broad eaves; c) Dormers with gable, hip, or shed roofs; d) Flat panel skylights or roof window on secondary elevations; e) Turrets or large projecting window bays used sparingly.

Section 14.025 (I) identifies the roofing elements discouraged.

1. All Building Types: a) False mansard or other applied forms; b) Dome skylights.

Finding: The structure will have a flat roof which incorporates a parapet wall, and large wood beam canopy. Flat roofs behind a parapet are encouraged design on commercial buildings. Other encouraged commercial roof designs include single gables with low pitch, repetitive fables with steep pitch or design which incorporate skylights. Adjacent commercial structures have a variety of roof designs and levels such as the CMH Cancer clinic. The awnings adds an element to break up the roof design while maintaining encouraged design elements for commercial structures. The signage near the roofline shall require



a sign permit, and should it extend about the roofline, it shall conform to dimension requirements in Article 8 for signage. This

guideline is met

- I. Section 14.025(J) identifies roofing materials encouraged.
  1. All Building Types: a) Cedar shingle; b) Composition roofing; c) Roofing material in gray, brown, black, deep red, or other subdued colors.
  2. Waterfront Industrial: a) Galvanized corrugated metal; b) Low profile standing seam, metal roof; c) Roll down.
  3. Commercial: a) Built-up.

Section 14.025(K) identifies roofing materials discouraged.

1. All Building Types: a) High profile standing seam, metal roof; b) Brightly colored roofing material.

Finding: The roofing material proposed is noted as white or gray Thermoplastic polyolefin (TPO), and heavy wooden brackets to support the canopy. The applicant shall clarify if the final proposed material is gray or white. Gray is the encouraged color to maintain a neutral or subdued roof design. If the material is a subdued color, this guideline has been met. At one point, the applicant discussed proposing solar panels on the roof. Should any solar panels or structures be incorporated into the design, the applicant shall submit a plans and a solar permit, required in Development Code article 16.

J. Section 14.025(L) identifies signs encouraged.

1. All Building Types: a) Hanging blade signs; b) Signs painted on building facade; c) Signs applied to building facade; d) Front lit; e) Graphics historic in character.
2. Commercial: a) Exterior neon.

Section 14.025(M) identifies signs discouraged.

1. All Building Types: a) Pole mounted freestanding signs; b) Plastic or internal and back lit plastic.

Finding: The applicant proposes aluminum letter signage on the east elevation near the main entrance. The applicant noted the co-op was exploring possibilities for a new logo and lettering. With that process is underway, the actual proposed signage is expected to change. The lettering below is a likely area where signage will be installed.

Signage and any advertising at the site will need a sign permit and will be reviewed against the signage requirements in Article 8 and Article 14 of the Development Code. Without additional details



on the proposed dimensions and design for the signage, this guideline is not met. At their discretion, the DRC may direct Community Development staff to review signage when it is submitted with a sign permit. If needed it may be

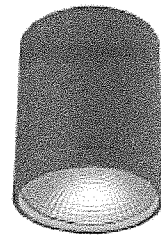
reviewed by the DRC as an amendment to an existing permit. Signage shall not be installed prior to review by staff and/or the DRC.

K. Section 14.025(N) identifies exterior lighting encouraged.

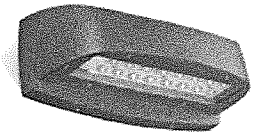
1. All Building Types: a) Decorative lighting integrated with architecture; b) Metal halide or incandescent; c) Pedestrian and traffic signals combined with street lamps; d) Light fixtures that direct light downward and eliminate glare.
2. Waterfront Industrial: a) Industrial pan light with goose neck; b) Low bollard lighting.
3. Commercial: a) Historic street lamps along walks and parking lots.

Section 14.025(O) identifies exterior lighting discouraged.

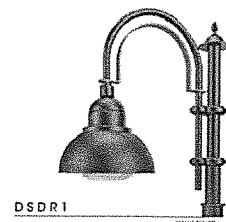
1. All Building Types: a) Sodium vapor (amber); b) Fluorescent tube; c) Cobra head street lamps or other contemporary fixtures; d) Fixtures with un diffused, undirected light that do not focus the light to the ground and that will potentially destroy the night sky view.



Finding: The parking light lighting plan is shown on page 35 of the submitted plans. Proposed fixtures are noted below. Additional decorative lighting integrated with the architecture and fixtures that direct light downward to eliminate glare are encouraged designs. The gooseneck lighting is preferred, to provide consistency with similar existing fixtures in the Mill Pond neighborhood. Bulbs shall not extend below the shroud of the lamp.



Two fixtures are proposed and shown to the right, and noted on the east elevation. The applicant shall clarify which fixtures are proposed for installation, and if they will be installed on any other facades. In an email dated 4-6-18, the applicant noted they will add four wall lights along the west wall. The applicant shall confirm where these lights will be located at the time of submitting a building permit.

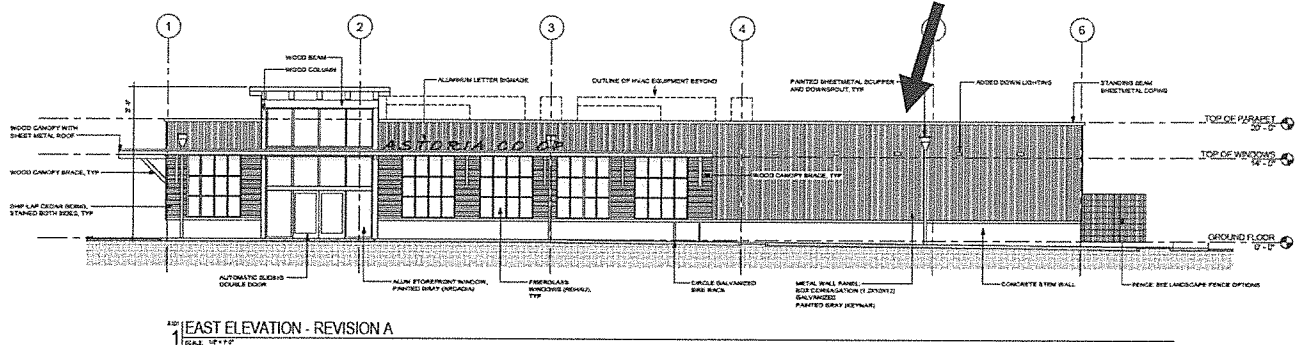


Any additional lighting shall be downcast. The goose neck like fixtures proposed meet the guidelines. Historic street lamps are also an encouraged design. The guideline is met.



**Specifications**

SPH.	0.56 sq
Length	28 1/2"
Width	16 3/4"
Height	45 3/4"
Weight	21 lbs



L. Section 14.025(P) identifies other design elements encouraged.

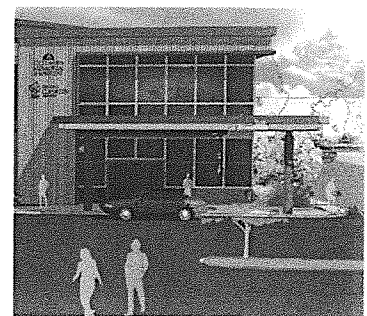
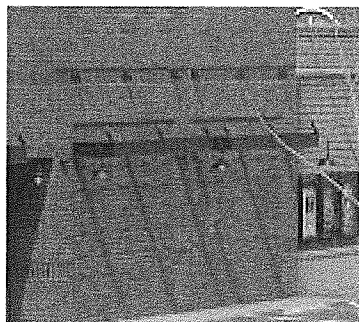
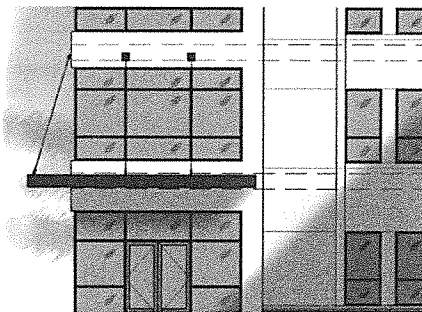
1. Commercial: a) Canvas awnings or fixed canopies for rain protection.

Section 14.025(Q) identifies other design elements discouraged.

2. Commercial: a) Vinyl awnings; b) Back lit awnings.

Finding:

The fixed wooden canopy for rain protection and ease of customer entry is encouraged in commercial buildings. The use of wood is encouraged, especially in this location which is near the historic wooden mill that once operated near the site. Examples of other successful awning designs are below. The wooden canopy meets the design criteria.



- M. Section 14.030(A)(1) concerning building orientation states that development projects should form visually continuous, pedestrian-oriented street fronts with no vehicle use area between building faces and the street. Exceptions to this requirement may be allowed to form an outdoor space such as a plaza, courtyard, patio, or garden between a building and a sidewalk. Such a larger front yard area should have landscaping, low walls, fencing, railings, a tree canopy, or other site improvements.

Finding: The proposed project covers a site area of almost 77,000 square feet. The building itself is approximately 11,580, with the main entrance on the east side of the structure facing the parking lot. Staff discussed relocating the building to other areas of the lot and changing the orientation. The applicant submitted alternative siting plans with previous submissions. The final site plan dated 4-24-18 shows the parking lot east of the building, with the entrance to the site off Steam Whistle Way. Public comment received and included in the Committee packets include testimony from neighboring property owners concerning the orientation of the building.

Incorporating outdoor seating on the south façade of the site as proposed on page 3 of the plans helps maintain a pedestrian friendly design. The use of this patio area between the buildings is allowed, to create a pedestrian oriented street front. However, with the current siting, there is a lack of visually continuous pedestrian-oriented street frontage along both Marine Drive and 23<sup>rd</sup> Avenue.

With additional design elements such as lighting would make for a more active and engaging frontage along Marine Drive, where neighboring business have their front locations facing. Decorate elements would help compensate for the lack of entrances or accessibility on the south and west facades with the proposed building orientation.

The design and orientation of the building should take more than just vehicular access from the parking lot into account. The neighboring areas are zoned for commercial, residential and family activities, all of which draw populations other than just retail sales customers to the area. The DRC shall provide a determination if the current proposed orientation of the building meets this criteria.

- N. Section 14.030(A)(2) concerning building orientation states that new uses should *"be sited to take advantage of the Columbia River and hillside views."*

Finding: The applicant notes orientation of the building has been addressed to take advantage of the Columbia River by locating the building on a north/south axis. The structure meets the setback requirement of 5' adhering to the view corridor requirements, and protecting views of the river. This guideline has been met.



- O. Section 14.030(A)(3) concerning building orientation states that if the proposed project is large or situated so as to become an entrance or major focus of the City, the design should recognize the project's prominence and should be both compatible with its surroundings and complementary to the City as a whole.

Finding: While the proposed project is only one story, with the current orientation and large footprint, it is a prominent structure as you enter the City from the east. The design and materials reflect the previous industrial uses of the Mill Pond area. This criteria has been met.

- P. Section 14.030(B)(1) concerning building massing states that buildings should have a floor area ratio on their lots of at least 1:1 (One square foot of building area for one square foot of lot area), in order to maximize use of the land.

Finding: the FAR criteria requirement is not required as the location falls in the Gateway and Greenway zone and the FAR does not apply to "on-land development in the Civic Greenway Overlay Zone within the Gateway Overlay Zone." This ratio requirement is waived.

- Q. Section 14.030(B)(2) concerning building massing states that buildings should be a minimum of 24 feet in height from grade to highest point of the structure, excluding those features exempt from building height as identified in Development Code Section 3.075.

Finding: The proposed building height is 20' to the parapet wall and 25'5" to the top of the awning, which meets the criteria of 24'. The LS Zone has a maximum height of 35' above grade per Article 2.980. The applicant has noted HVAC equipment on the roof, but has not submitted dimensions or if they propose to screen any of the equipment. Additional informational shall be submitted regarding the HVAC system on the roof including dimensions and location.

- R. Section 14.030(B)(3) concerning building massing states that the height, mass, and scale of buildings should be compatible with the site and adjacent buildings. Use of materials should promote harmony with surrounding historic structures and the character of the waterfront.

Finding The proposed structure is one-story. There are one, two, and three story commercial buildings in this area. Buildings along Exchange Street include the hospital and OSU Seafood Center which are large, multi-story buildings. One of the tallest commercial buildings in the overlay is the Fire Fighters Museum at 30<sup>th</sup> and Marine Drive, which is a flat roofed building at approximately 38' high. The remaining buildings are mostly one and two stories with a few three story residential structures.

The height, mass and scale of the building is in proportion with the large site on which it is located. Without the awning element, at less than 24', the building risks looking like a structure that is out of scale with the neighboring developments. The wooden awning provides a design element that ties into the historic character of the Mill Pond area. This guideline is met.

- S. Sections of the Comprehensive plan concerning the Gateway Overlay Area include: CP.058. Gateway Overlay Area Policies. This section states that the City will utilize the general vision of the Gateway Master Plan to direct future development in the Gateway Overlay Area. The overall Comprehensive Plan objectives are to:
- a. promote development that complements the Downtown Area;
  - b. enhance the primary uses, such as the Columbia River Maritime Museum and Columbia Memorial Hospital, and work to redevelop areas such as the former John Warren Field site, which has significant development potential;
  - c. promote new land uses complementary to the riverfront and existing development, particularly visitor oriented uses and high density housing;
  - d. establish visual and physical linkages within and around the Gateway Overlay Area, with special emphasis on the Columbia River riverfront;
  - e. create a pedestrian-friendly environment throughout the Gateway Overlay Area through the careful siting of buildings and parking lots, careful consideration of street frontage design, and extension of the Astoria River Trail; and
  - f. create investor interest by promoting complementary land uses and quality development in the surrounding area.

Finding: The proposed development is a relocation and expansion of an existing use in the Downtown Area. The relocation will provide easier access for hospital workers in the area, and Mill Pond residents. This parcel on the corner of 23d and Marine drive was intended for commercial use when the Mill Pond area was first developed. The proposal does not necessarily create a pedestrian friendly environment with the street frontage design, however the proposed development does provide an opportunity for a visitors and local residents to utilize the undeveloped lot. The proposal is also in compliance with economic development goals listed in the Comprehensive Plan CP.210 recommending promoting a combination of tourist oriented development and services for Astoria residents and businesses. A co-op grocery as a retail sales establishment supports this goal by providing opportunities for tourists and local residents.

## V. CONCLUSION AND RECOMMENDATION

The request meets most the Design Review Guidelines, with additional information, many of the outstanding criteria items can be met. Should the Design Review Committee approve the current proposal, the following issues shall be addressed with staff. If these issues cannot be resolved, the applicant may resubmit materials for review by the DRC.

To approve the request, it is recommended the DRC consider the Findings of Fact, and recommendations below. The DRC may choose to add additional conditions of approval.

The applicant should be reminded of the following issues:

1. The DRC shall provide a determination if the current proposed orientation of the building meets this criteria. In previous submittals, the applicant outlined alternative building orientation. This siting has been included in testimony submitted by a number of Mill Pond homeowners in a letter dated 4/24/18.
2. The applicant shall submit a recorded document with a lot line adjustment permit to the Community Development Department prior to submitting building permits.
3. Additional detailing on proposed doors shall be submitted by the applicant.
4. The applicant shall confirm that glass in all proposed windows are clear and not frosted or spandrel. Any windows with grids shall be true divided or shall have external grids.
5. The applicant shall confirm the reveal dimensions for the proposed siding.
6. The applicant shall clarify if the final proposed material is gray or white. Gray is the encouraged color to maintain a neutral or subdued roof design.
7. Should the applicant wish to incorporate solar panels, plans and a permit shall be submitted for review by the Community Development department for compliance with Article 16 of the development code.
8. Signage proposed will need a separate sign permit, and shall be reviewed against the Sign Ordinance, and compliance with the overlay zones.
9. The applicant has noted this lighting on the east elevation. The applicant shall clarify which fixtures are proposed for installation, and if they will be installed on any other facades including 4 lights located on the west façade as noted in an email dated 4-6-18.
10. A design for trash enclosure, and any noise abatement design elements shall be submitted to the Community Development Department for review.

11. Additional informational shall be submitted regarding the HVAC system on the roof including dimensions and location.
12. Any changes to the landscape plan shall be submitted for review and approval of the Planner at the time of the building permit application. Landscaping shall be installed prior to occupancy of the building.
13. Any change in design or material or modifications to the proposed plans as described in this Staff Report shall be submitted to the Community Development Department for review.
14. The applicant shall obtain all necessary City and building codes permits, including a utility service application, grading and erosion control permit, Right of way Permit and Construction Permit.



CITY OF ASTORIA  
Founded 1811 • Incorporated 1856  
COMMUNITY DEVELOPMENT

CITY OF ASTORIA  
NOV 28 2017  
BUILDING CODES

DR 17-03

☒ Fee Paid Date 11/30/17 By DM nf  
Fee: \$750.00

DESIGN REVIEW >25,000 Project Value

Property Address: 2350 Marine Dr. Astoria  
Lot 4A Block 144 Subdivision Shivelys  
Map 9CB Tax Lot 6803 Zone AH-MP \* Pending zone change  
Applicant Name: Astor Venture, LLC, Garry Vallaster  
Mailing Address: P.O. Box 3933, Portland, Or. 97208  
Phone 503-880-4570 Email: donv@vcarh.com  
Property Owner's Name: Astor Venture, LLC / Agent - Garry Don Vallaster  
Mailing Address: P.O. Box 3933, Portland, Or. 97208  
Phone: 503-880-4570 Email: \_\_\_\_\_  
Signature of Applicant: [Signature] Date: 11.8.17  
Signature of Property Owner: 11 Date: 11  
Proposed Construction: Wood frame construction on structural concrete slabs.  
Site Dimensions & Square Footage: Approx 250' x 182', 45,452 s.f.  
Building Square Footage: 1st Floor: 11,580 sf 2nd & 3rd Floor: \_\_\_\_\_ Garage: \_\_\_\_\_  
Accessory Building Information: \_\_\_\_\_

**FILING INFORMATION:** The Design Review Committee meets on the first Thursday of the month, as needed depending on date of applications. Complete applications must be received by the 1<sup>st</sup> of the previous month. A pre-application meeting with the Planner is required prior to the acceptance of the application as complete. Only complete applications will be scheduled on the agenda. Your attendance at the Design Review Committee meeting is recommended.

For office use only:			
Application Complete:	<u>11/30/17 DM</u>	Permit Info Into D-Base:	
Labels Prepared:	<u>Withdrawn &amp; resubmitted</u>	Tentative DRC Meeting Date:	<u>4/19 5/3/18</u>
120 Days:			

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BUILDING CODES

All information concerning construction materials, design, dimensions, etc. is REQUIRED. If submitting large format plans, please also submit a reduced copy at 11" x 17" for reproducing.

Briefly address each of the Design Review Guidelines and state whether the project complies with the guideline, if applicable, and why this request should be approved. Please provide manufacturer information and/or detailed information for use of any material or design not selected from the "Encouraged" list in the Design Guidelines. (Use additional sheets if necessary.):

## 1. Building Form.

Basic Shape: The building is a simple rectangle 86' x 128'

Porches & Balustrade - Design, Dimension, Features, Materials: N.A.

Balconies & Balustrade - Design, Dimension, Features, Materials: \_\_\_\_\_

Other: The building has protective canopies on the east & south facing building sides.

## 2. Windows.

Material: Windows will be aluminum or fiberglass

Divided Windows (true divided, external muntins, etc): \_\_\_\_\_

Operation (casement, single hung, etc.): Will be a combination of fixed and awning windows

Size & Material of Exterior Casings (minimum 5/4" x 4"; provide detail diagram): \_\_\_\_\_

This a commercial building without window casings

Other: \_\_\_\_\_

## 3. Exterior Wall Treatments.

Material & Dimensions of Siding (note if material is smooth or textured): Exterior walls to be 22-Ga AEP steel box rib. Flex series 12x10-12

Decorative Features: walls on the east & south sides under the canopy will be cedar or fir.

Other: \_\_\_\_\_

## 4. Doors.

Material & Design: The main entry doors are glass/aluminum other doors to be hollow metal or fiberglass.

Other: \_\_\_\_\_

## 5. Roof Elements.

Style and Pitch of Roof: Flat roof sloped for drainage with a raised roof in the entry area

Material: TPO

Color: white or gray

Decorative Features (eave brackets, etc): Heavy wood brackets will support the canopies.

Other: \_\_\_\_\_

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6. **Garage.**  
 Garage Door Material & Design: N.A. There will be (1) BUILDING CODES  
 Window Material & Design: door in delivery area  
 Roof Style & Material: \_\_\_\_\_  
 Other: \_\_\_\_\_
7. **Signs.**  
 Dimension & Square footage: To be submitted for separate review  
 Location: \_\_\_\_\_  
 Type, Material & Design: To be determined  
 Other: \_\_\_\_\_
8. **Exterior Lighting.**  
 Fixture & Lamp Design: Lithonia MRI LED,  
 Location: parking lot. Pole mounted  
 Other: Lighting under the canopy to be determined
9. **Other Design Elements.**  
 (Fences, out buildings, corner boards, belt course, etc. with dimensions): \_\_\_\_\_  
The delivery yard fencing will match the building siding
10. **Building Orientation.**  
The building long dimension will be on a North/South axis
11. **Building Massing.**  
 Building to Lot Ratio: It is at a 3.9:1 ratio. Lot coverage is @ 25%  
 Other: of lot area. 45,452 sf. lot & 11,580 sf. building
12. **Access and Parking Design.**  
 Number of Off-street Spaces: 50 spaces in the parking lot w/  
 Other: several spaces for deliveries in the loading area
13. **Landscaping.**  
Extensive. See landscape drawing
14. **Underground Utilities.**  
Electric, Gas, water, communications

**PLANS:** A site plan indicating location of the proposed structure on the property is required. Diagrams showing the proposed construction indicating style and type of materials proposed to be used are required. Scaled free-hand drawings are acceptable. The City may be able to provide some technical assistance on your proposal if it is adjacent to a historic structure and will require additional review by the Historic Landmarks Commission.

If submitting large format plans, please also submit a reduced copy at 11" x 17" for reproducing.

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BUILDING CODES

**LS: LOCAL SERVICE ZONE**

**2.976. USES PERMITTED OUTRIGHT.**

The following uses and their accessory uses are permitted in an LS Zone if the Community Development Director determines that the uses will not violate standards referred to in Sections 2.978 through 2.981, additional Development Code provisions, the Comprehensive Plan, and other City laws:

6. Retail sales establishment. (Retail sales establishment greater than 14,000 gross square feet shall include a minimum of 50% of gross floor area devoted to multi-family dwellings located above the first floor.)

**Applicant Finding:** Upon adoption of the proposed text and map amendments the retail sales establishment will be an outright use.

**2.978. LOT COVERAGE.**

Buildings will not cover more than 80% of the lot area.

**Applicant Finding:** Based on the building size of 11,580 square feet, and a final lots size of 45,451 square feet the lot coverage will be approximately 25%, far less than the 80% maximum. This standard is met.

**2.979. LANDSCAPED OPEN AREA.**

A minimum of 20% of the total lot area will be maintained as a landscaped open area.

**Applicant Finding:** The lot size will be 45,451 square feet with 9,584 square feet landscaped. The total landscaped area will be 21.1% of the total lot area.

**2.980. HEIGHT OF STRUCTURES.**

No structure will exceed a height of 35 feet above grade.

**Applicant Finding:** As demonstrated in the attached drawings (Exhibit 1) the building height will not exceed 28 feet as required by the Civic Greenway Overlay Zone, except as allowed by Section 3.075.

**2.981. OTHER APPLICABLE USE STANDARDS.**

1. Landscaping shall meet the requirements of Sections 3.105 through 3.120.

**Applicant Finding:** As demonstrated in the landscape improvement plan the requirements of 3.105 through 3.120 are met.

2. Outdoor storage areas will be enclosed by appropriate vegetation, fencing, or walls. This requirement does not apply to outdoor retail sales areas.

**Applicant Finding:** As demonstrated in the attached drawings outdoor storage (loading bay) areas will be enclosed. This standard is met.



**ARTICLE 3**

**ADDITIONAL USE AND DEVELOPMENT STANDARDS**

**3.105. LANDSCAPING.**

**3.110. LANDSCAPING REQUIRED.**

At the time a building permit is requested for new construction, or for remodeling with a value of at least 33% of the assessed value of the structure, or in the event of a change of use or installation of new parking areas, the property shall come into compliance with the landscape requirements and a landscaping plan shall be submitted to the Community Development Director. Such landscaping plan may also be used as a site or plot plan for the development, provided all information necessary for the site or plot plan is provided. The plan shall be of sufficient scale to show existing and proposed features, proposed materials, contours (where appropriate) and other features.

**Applicant Finding:** A landscaping improvement plan is included with the application materials.

**ARTICLE 7**

**OFF-STREET PARKING AND LOADING**

**7.030. LOCATION.**

**Applicant Finding:** All parking is located on site.

**7.100. MINIMUM PARKING SPACE REQUIREMENTS.**

Table 7.100 – Off-Street Parking Space Requirements by Use.

Retail Sales, General Merchandise:	1 space per 500 sq. ft. gross floor area
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**Applicant Finding:** The site plan identifies 50 parking spaces which far exceeds the Table 7.100 requirement of 23 spaces for an 11,580-square foot building. These parking spaces will also be used for deliveries by smaller trucks.

**7.105. BICYCLE PARKING.**

Table 7.105: Minimum Required Bicycle Parking Spaces

Commercial:	1 bike spaces per primary use or 1 per 10 vehicle spaces, whichever is greater
-------------	--

**Applicant Finding:** The site plan indicates that three bicycle spaces will be installed. This number coincides with the required 23 car parking spaces in Table 7.100.

**7.110. PARKING AND LOADING AREA DEVELOPMENT REQUIREMENTS.**

**Applicant Finding:** The Applicant will adhere to all parking and loading area development requirements. This can be guaranteed by a condition of approval.

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3. Where feasible, joint access points and parking facilities for more than one use should be established. This standard does not apply to multi-family residential developments. BUILDING CODES

Applicant Finding: Not applicable at this time.

4. Access drives and parking areas should, where possible, be located on side streets or non-arterial streets in order to minimize congestion on Marine Drive.

Applicant Finding: All access points are located on side or non-arterial streets, 23<sup>rd</sup> and Steam Whistle Way. This standard is met.

5. All uses will comply with access, parking, and loading standards in Article 7.

Applicant Finding: Please refer to the findings for Article 7. This requirement is met.

6. Conditional uses will meet the requirements in Article 11.

Applicant Findings: This standard is not applicable.

7. Signs will comply with requirements in Article 8 and specifically, the Specific regulations of the C-3 Zone in Section 8.150.

Applicant Findings: Please refer to the findings in for Article 8.

8. All structures will have storm drainage facilities that are channeled into the public storm drainage system or a natural drainage system approved by the City Engineer. Developments affecting natural drainage shall be approved by the City Engineer.

Applicant Findings: The Applicant has not yet developed a storm water drainage plan. This can be required as a condition of approval.

9. Where new development is within 100 feet of a known landslide hazard, a site investigation report will be prepared by a registered geologist. Recommendations contained in the site report will be incorporated into the building plans.

Applicant Finding: The development is not known to be within 100 feet of a known landslide area; however, a site investigation report will be completed as part of the Mill Pond development requirements.

10. All uses will comply with the requirements of the Gateway Overlay Zone in Sections 14.005 to 14.030.

Applicant Finding: Please refer to the findings for the Gateway Overlay Zone. This standard is met.

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7.160. MINIMUM LOADING SPACE REQUIREMENTS.  
C. Commercial, Non-office, Public and Semi-Public

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<u>Floor Area</u>	<u>Minimum Number of Spaces</u>	<u>Minimum Size of Space</u>		
5,000 - 59,999 sq ft	1	12 ft	19 ft	12 ft

Applicant Finding: The proposed loading area (69'-0" x 39'-6") exceeds this requirement. The standard is met.

7.170. LANDSCAPING OF OUTDOOR STORAGE OR PARKING AREAS.

A minimum of 5% of the gross parking lot area shall be designed and maintained as landscaped area, subject to the standards in Sections 3.105 through 3.120. This requirement shall apply to all parking lots with an area of 600 square feet or greater. Approved sight obscuring fences or vegetative buffers shall be constructed where commercial parking lots abut Residential Zones. The minimum 5% landscaping shall be counted as part of the total landscaping required for the property.

Applicant Finding: As demonstrated in the attached drawings the parking lot will be 18,187 square feet in size with the landscaped area being 1,107. Therefore, 6.1 % of the parking area is landscaped.

ARTICLE 8  
SIGN REGULATIONS

Applicant Finding: The Astoria Co-op Grocery has provided a draft of sign design and locations on the site. *Refer to Exhibit 2.* The Co-op is in the process of creating a new logo which is not yet available. All signs will adhere to the requirements of Article 8 and the Gateway Overlay Zone.

**GO: GATEWAY OVERLAY ZONE**

Design Review Criteria  
Building Forms

Applicant Finding: The building is rectangular in shape and low in form.

Windows

Applicant Finding: The Applicant proposes aluminum or fiberglass fixed and awning windows.

Exterior Wall Treatments

Applicant Finding: Exterior walls will be 22-gauge AEP steel box rib. Flex series 1.2x10-12. Walls on the East and South sides under the canopy will be cedar or fir.

Doors

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Applicant Finding: The main entry doors are glass and aluminum. Other doors will be fiberglass. BUILDING CODES

#### Roof Elements

Applicant Finding: A flat roof sloped for drainage is proposed with TPO roofing materials. The roof above the entryway will be raised.

#### Garage

Applicant Finding: No garage doors are proposed. A door will be in the delivery bay.

#### Signs

Applicant Finding: The Astoria Co-op Grocery has provided a draft of sign design (Exhibit 2) and locations on the site. The Co-op is in the process of creating a new logo which is not yet available. All signs will adhere to the requirements of Article 8 and the Gateway Overlay Zone.

#### Exterior Lighting

Applicant Finding: The Applicant will install Lithonia MRI LED pole mounted in the parking lot. Lighting under the canopy is currently undetermined.

#### Other Design Elements

Applicant Finding: The delivery bay fencing will match the building siding.

### 14.030. OTHER APPLICABLE USE STANDARDS

#### A. Building Orientation.

1. Development projects should form visually continuous, pedestrian-oriented street fronts with no vehicle use area between building faces and the street.
  - a. Exceptions to this requirement may be allowed to form an outdoor space such as a plaza, courtyard, patio, or garden between a building and a sidewalk. Such a larger front yard area should have landscaping, low walls, fencing, railings, a tree canopy, or other site improvements.
2. New uses should be sited to take advantage of the Columbia River and hillside views.
3. If the proposed project is large or situated so as to become an entrance or major focus of the City, the design should recognize the project's prominence and should be both compatible with its surroundings and complementary to the City as a whole.

Applicant Finding: The building is oriented so that the long dimension is on a North-South axis. Pedestrian access is available from Marine Drive and Steam Whistle Way to the entrance. The site will be heavily landscaped and will have hillside views.

#### B. Building Massing.

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1. Buildings should have a floor area ratio on their lots of at least 1:1 (One square foot of building area for one square foot of lot area), in order to maximize use of the land.

Applicant Finding: The proposed Co-op building covers approximately 25% of the lot which does not meet this standard. However, Civic Greenway Overlay Zone Section 14.070.A.1., which also applies to this development, provides that floor area ratios in the Gateway Overlay does not apply in the Civic Greenway Overlay. This standard is not applicable.

2. Buildings should be a minimum of 24 feet in height from grade to highest point of the structure, excluding those features exempt from building height as identified in Development Code Section 3.075.

Applicant Finding: The proposed Co-op building covers may exceed 24 feet in height which does not meet this standard. However, Civic Greenway Zone Section 14.070.A.1., which also applies to this development, provides that the height standard in the Gateway Overlay does not apply in the Civic Greenway Overlay. This standard is not applicable.

3. The height, mass, and scale of buildings should be compatible with the site and adjacent buildings. Use of materials should promote harmony with surrounding historic structures and the character of the waterfront.

Applicant Finding: The structure will only cover approximately 25% of the final lot size and will have extensive landscaping. This will ensure that the development is harmonious with surrounding structures and the waterfront.

#### C. Access and Parking Design.

1. All uses which are served by an alley, local street, or collector street should have alley or street vehicular access and egress. Curb openings onto Marine Drive or Exchange Streets are discouraged. Parking lots should be on the interiors of blocks or behind buildings, and should be designed to be as unobtrusive as possible.

Applicant Finding: Appropriate ingress and egress is provided by Steam Whistle Way and no curb openings will occur on Marine Drive. The parking lot is not located behind the building but will be landscaped to ensure it is not obtrusive or unattractive.

2. Building facades and entries should face the adjacent street. Main entrances should face a connecting walkway with a direct pedestrian connection to the street without requiring pedestrians to walk through parking lots or across driveways.

Applicant Finding: The building façade does not face the street but the parking lot. Pedestrian access is available from Marine Drive and Steam Whistle Way so that pedestrians will not have to cross parking lots or driveways. This also provides for a view corridor as required in the Civic Greenway Overlay Zone. The intent of this criterion is met.

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3. Parking areas should be shared among various uses where a development or block is planned as a whole. On-street parking on internal streets may be counted towards the off-street parking requirements with the approval of the Community Development Director.

**Applicant Finding:** No on-street parking is utilized and there are no adjacent uses in which to share parking. This standard does not apply.

**D. Landscaping.**

1. Street trees should be planted within the right-of-way along both sides of the streets within the Gateway Overlay Zone.
  - a. Spacing should be 30 feet on center, depending on species and branching habit.
  - b. Minimum size of deciduous trees should be 2" caliper, with an upright form.
  - c. Mature branching height should be a minimum of 15'.
  - d. Durable tree grates and trunk protectors should be installed.
2. Areas between trees should be landscaped with a variety of shrubs and perennials, with an emphasis on flowering species.

**Applicant Finding:** As demonstrated on the attached landscape improvement plan the Co-op site will have extensive landscaping and will adhere to the above requirements.

**E. Underground Utilities.**

This provision shall apply only to utility lines to be installed for new construction. Utility lines, including, but not limited to, electricity, communications, street lighting and cable television, shall be required to be placed underground. Appurtenances and associated equipment such as surface-mounted transformers, pedestal mounted terminal boxes and meter cabinets may be placed above the ground, and shall be screened by sight obscuring fences and/or dense landscape buffers. The Design Review Committee may waive the requirements of this section if topographical, soil, or other conditions make such underground installations or screening of above ground equipment unreasonable or impractical. The applicant shall make all necessary arrangements with the serving utility or agency for underground installations provided hereunder; all such installations shall be made in accordance with the tariff provisions of the utility, as prescribed by the State Public Utilities Commissioner.

**Applicant Finding:** All utilities will be placed underground and will conform to the applicable standards.

**CGO: CIVIC GREENWAY OVERLAY ZONE**

**14.060. STANDARDS FOR ON-LAND DEVELOPMENT.**

**A. Height.**

1. Maximum building height is 28 feet.

**Applicant Finding:** The building height will not exceed 28 feet as required by the Civic Greenway Overlay Zone, except as allowed by Section 3.075. This standard is met.

**B. Setbacks.**

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A minimum view corridor width of 70 feet, centered on the right-of-way centerline, shall be provided on north-south rights-of-way between Marine Drive/Lief Erikson Drive and the Columbia River. Buildings shall be set back in order to achieve the 70- foot view corridor.

**Applicant Finding:** The building height is set back at the west end of the lot to allow an approximate 119-foot view corridor across the parking lot. This standard is met.

14.070. OTHER DEVELOPMENT STANDARDS.

A. The following development standards are applicable within the Civic Greenway Overlay Zone.

1. Floor area ratios.

Floor area ratio and height standards in Section 14.030(B)(1) and Section 14.030(B)(2) of the Gateway Overlay Zone do not apply to on-land development in the Civic Greenway Overlay Zone. Other use standards in Section 14.030 apply.

**Applicant Finding:** As demonstrated in findings for the Gateway Overlay Zone the applicant understands the standards in Section 14.030(B)(1) and Section 14.030(B)(2) do not apply.

2. Exterior lighting.

Outdoor lighting shall be designed and placed so as not to cast glare into adjacent properties. Light fixtures shall be designed to direct light downward and minimize the amount of light directed upward. The Community Development Director may require the shielding or removal of such lighting where it is determined that existing lighting is adversely affecting adjacent properties or contributing to light directed into the night sky.

**Applicant Finding:** The proposed exterior lighting (Exhibit 3) is designed to meet this requirement. The standard is met.

3. Fences.

Fences located between the River Trail and the Columbia River shall not exceed a height of three (3) feet.

**Applicant Finding:** This standard is not applicable.

14.075. LANDSCAPING.

A. Landscaping is required in the Civic Greenway Overlay Zone in accordance with the provisions in this Section and those in Sections 3.120 to 3.125. The provisions in this Section apply to new construction or exterior renovations with a value of at least 20% of the assessed value of the structure, or in the event of installation of new parking areas.

Street Trees.

a. Street trees are required to be planted within the right-of-way along both sides of the street in the Civic Greenway Overlay Zone in accordance with the provisions in this Section and those in Section 14.030(D). Maximum height for street trees along north-south streets between Marine Drive and the Columbia River is 45 feet.

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- b. Street trees along north-south streets between Marine Drive and the Columbia River shall have narrow profiles and/or be pruned to a maximum width of 15 feet.
- c. Street trees along north-south streets between Marine Drive and the Columbia River shall be one of the columnar species listed in Section 3.125, unless otherwise approved by the Community Development Director.
- d. Required street trees shall be maintained by the adjacent property owner and/or other identified entity. There shall be a maintenance agreement or other City approved agreement.

Applicant Finding: A landscaping plan is provided and meets the above requirements.



# Untitled Map

Write a description for your map.

## Legend

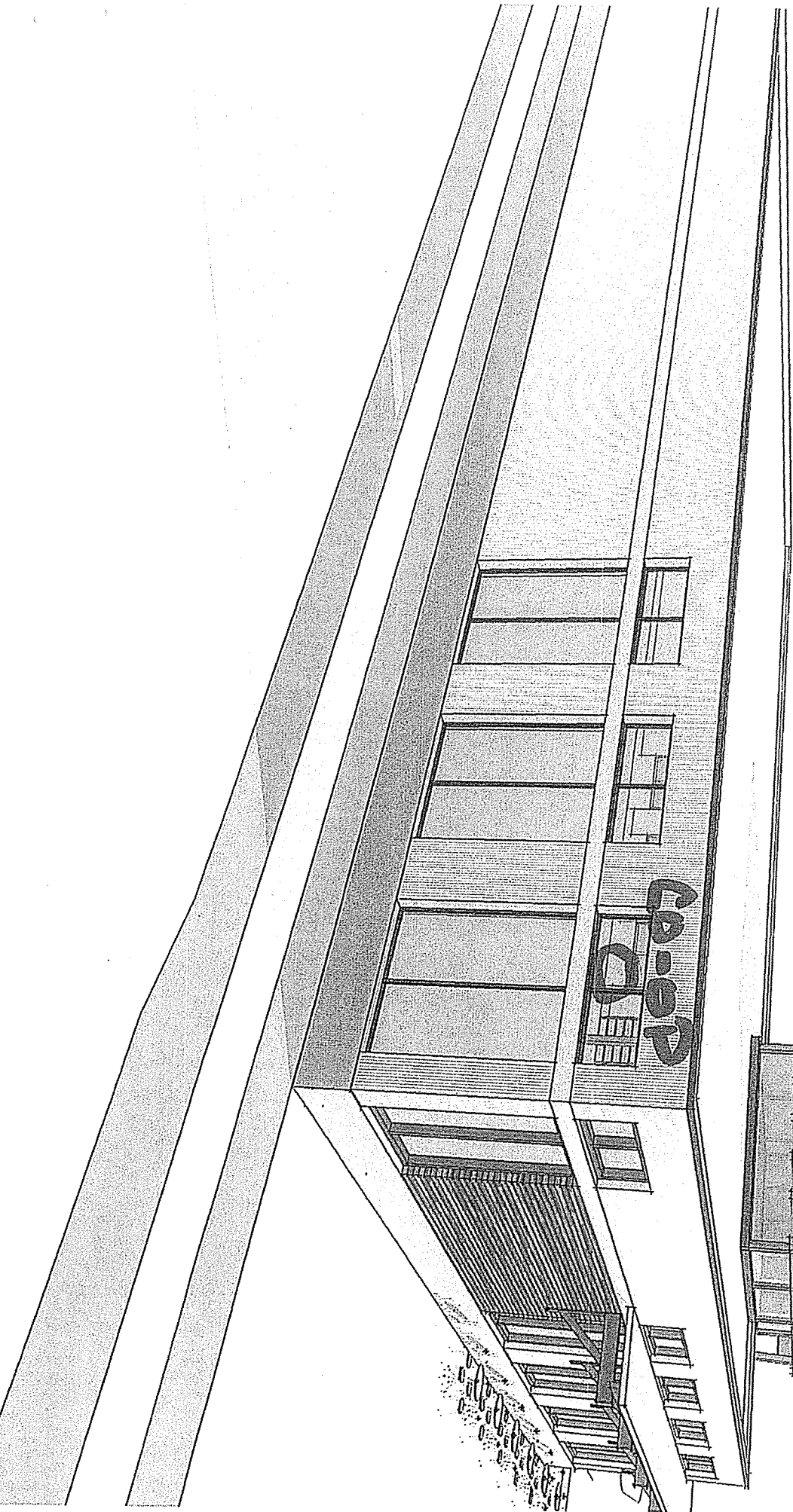


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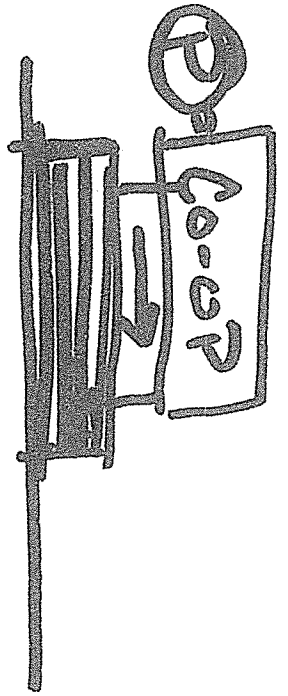
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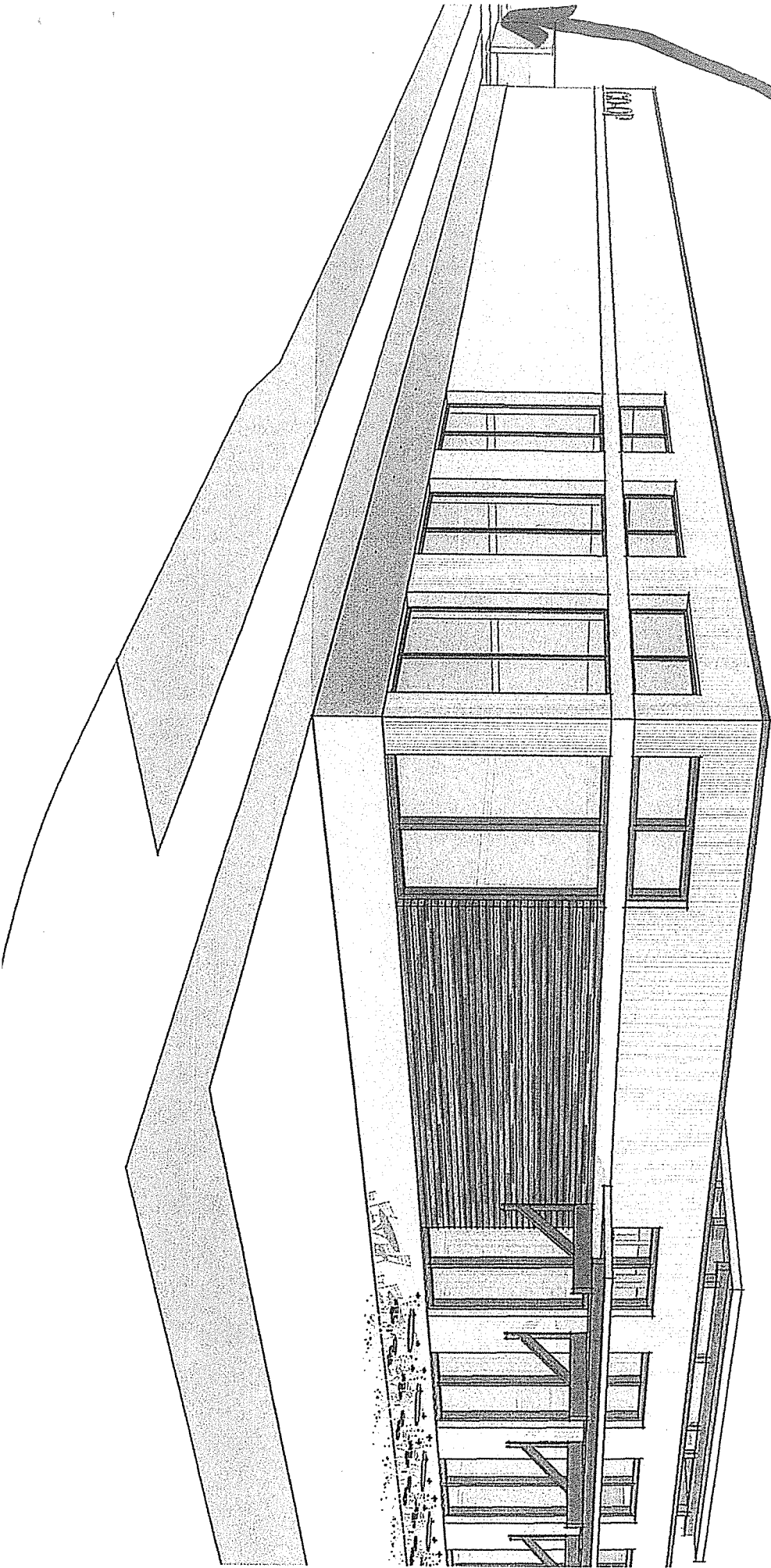
SECONDARY - BACK LOT OR  
HARD ACID/ST  
MATERIAL SURFACE



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CITY OF ASTORIA  
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BUILDING CODES





mailed 4/10/18

YOU ARE RECEIVING THIS NOTICE BECAUSE THERE IS A  
PROPOSED LAND USE APPLICATION NEAR YOUR PROPERTY IN ASTORIA

CITY OF ASTORIA  
NOTICE OF PUBLIC HEARING

Published  
4-27-18

The City of Astoria Design Review Committee will hold a public hearing on Thursday, May 3, 2018 at 5:30 p.m., in the Astoria City Hall Council Chambers, 2<sup>nd</sup> Floor, 1095 Duane Street, Astoria. The purpose of the hearing is to consider the following request(s):

1. Design Review DR17-03 by Garry Vallaster, Astor Venture, LLC to construct an approximately 11,580 square foot commercial building at 2350 Marine Drive (Map T8N-R9W Section 9CB; portion of Tax Lot 6803; portion of Block 144, Shively's, within the Gateway and Civic Greenway Overlay Zone in the LS (Local Services) Zone. Development Code Standards specified in Sections 2.975 to 2.981 (LS Zone) 14.005-14.030 (Gateway Overlay), 14.035-14.075 (Civic Greenway Overlay), Article 7, 8, 9, and Comprehensive Plan Sections CP.005 to CP.028, CP.057-CP.058 (Gateway Area Plan), CP.067 to CP.068 (Riverfront Vision Overlay), CP.070 to CP.075 (Uppertown Area), and CP.190-CP.210 (Economic Element) are applicable to the request.

A copy of the application, all documents and evidence relied upon by the applicant, the staff report, and applicable criteria are available for inspection at no cost and will be provided at reasonable cost. A copy of the staff report will be available at least seven days prior to the hearing and are available for inspection at no cost and will be provided at reasonable cost. All such documents and information are available at the Community Development Department at 1095 Duane Street, Astoria. If additional documents or evidence are provided in support of the application, any party shall be entitled to a continuance of the hearing. Contact the Planner at 503-338-5183 for additional information.

The location of the hearing is accessible to the handicapped. An interpreter for the hearing impaired may be requested under the terms of ORS 192.630 by contacting the Community Development Department at 503-338-5183 48 hours prior to the meeting.

All interested persons are invited to express their opinion for or against the request(s) at the hearing or by letter addressed to the Design Review Committee, 1095 Duane St., Astoria OR 97103. Testimony and evidence must be directed toward the applicable criteria identified above or other criteria of the Comprehensive Plan or land use regulation which you believe apply to the decision. Failure to raise an issue with sufficient specificity to afford the Design Review Committee and the parties an opportunity to respond to the issue precludes an appeal based on that issue.

The Design Review Committee's ruling may be appealed to the City Council by the applicant, a party to the hearing, or by a party who responded in writing, by filing a Notice of Appeal within 15 days after the Design Review Committee's decision is mailed. Appellants should contact the Community Development Department concerning specific procedures for filing an appeal with the City. If an appeal is not filed with the City within the 15 day period, the decision of the Design Review Committee shall be final.

The public hearing, as conducted by the Design Review Committee, will include a review of the application and presentation of the staff report, opportunity for presentations by the applicant and those in favor of the request, those in opposition to the request, and deliberation and

Don Vallaster

Åstor Venture, LLC  
PO Box 3933  
Portland OR 97208

Norma Hernandez  
92335 Youngs River Rd  
Astoria OR 97103

Matt Stanley  
664 Kensington Ave  
Astoria OR 97103

Jennifer Bunch  
Wickiup Consulting, LLC  
PO Box 1455  
Astoria Or 97103

Arlie Jensen  
1885 Huckleberry Dr.  
Seaside OR 97138

Misha Cameron-Lattek  
1820 SE 3<sup>rd</sup> St  
Astoria OR 97103

Jim Wolcott  
2735 Mill Pond Ln  
Astoria OR 97103

Don Vallaster

Åstor Venture, LLC  
PO Box 3933  
Portland OR 97208

Norma Hernandez  
92335 Youngs River Rd  
Astoria OR 97103

Matt Stanley  
664 Kensington Ave  
Astoria OR 97103

Jennifer Bunch  
Wickiup Consulting, LLC  
PO Box 1455  
Astoria Or 97103

Arlie Jensen  
1885 Huckleberry Dr.  
Seaside OR 97138

Misha Cameron-Lattek  
1820 SE 3<sup>rd</sup> St  
Astoria OR 97103

Jim Wolcott  
2735 Mill Pond Ln  
Astoria OR 97103

4/9/18

80909CB06839  
Aspmo Beverly J  
Aspmo Gary S  
371 W Grand Ave  
Astoria, OR 97103-6413

80909CB06803  
Astor Venture LLC  
Garry Don Vallaster  
711 SW Alder St PH  
Portland, OR 97205-3429

80909CB06861  
Astoria Gateway Associates II  
Cascade Management Inc  
13221 SW 68th Pkwy Ste #310  
Portland, OR 97223

80909CB04600  
Blackstock Delane E  
Blackstock Justin G  
2947 NE 60th Ave  
Portland, OR 97213

80909CB04401  
Blackstock Delane E  
Blackstock Justin G  
2742 Grand Ave #A  
Astoria, OR 97103

80909CB06847  
Bower Barbara A  
2410 N Aurora Ave Apt #108  
Seattle, WA 98109-2268

80909CB02800  
Chen Rong Wo  
Ma Xian Xiu  
2813 Marine Dr  
Astoria, OR 97103-2819

80908DD06800  
Columbia Lutheran Charities  
Columbia Memorial Hospital dba  
2111 Exchange St  
Astoria, OR 97103-3329

80909CB06843  
Darby Richard  
Darby Sandra  
2555 Mill Pond Ln  
Astoria, OR 97103-3303

80909CB06834  
Fransen Toivo J  
2795 Mill Pond Ln  
Astoria, OR 97103

80909CB04900  
Hageman Kenneth D/Beverly J  
2770 Grand Ave  
Astoria, OR 97103-2906

80909CB06809  
Hall David L  
275 23rd St  
Astoria, OR 97103-2229

80909CB06841  
Hankins Ronald Ben Rev Liv Trst  
Hankins Lois J Rev Liv Trust  
1720 SE Hawthorne Blvd  
Portland, OR 97214-3723

80908DA01402  
Heestand Family LLC  
PO Box 669  
Scappoose, OR 97056-0669

80908DA01401  
Heestand Family LLC  
T P Freightlines Accounts Payable  
PO Box 580  
Tillamook, OR 97141-0580

80909CB06810  
Iverson Trust  
285 23rd St  
Astoria, OR 97103-2229

80909CB06846  
Jarvis Ben L  
Jarvis Sally A  
5756 SW Cupola Dr  
South Beach, OR 97366

80909CC00600  
Johnson Eva M  
12321 Highway 99 #125  
Everett, WA 98204-5511

80909CC00100  
Kalander Sandra K  
Kalander John R  
1975 Cedar St  
Astoria, OR 97103-2115

80909CB06812  
Keown Riley M  
2231 Saybrook Ln  
Costa Mesa, CA 92627-1677

80909CB04100  
Kiepke R J  
2703 Marine Dr  
Astoria, OR 97103-2900

80909CB04300  
Kiepke R J  
1779 George Hill Rd  
Astoria, OR 97103-8274

80909CC00500  
Leino George E  
88338 Youngs River Rd  
Astoria, OR 97103-8127

80909CC00601  
Moore Ernest R Sr/Delores J  
503 N 1st St Spc #57C  
El Cajon, CA 92021

80909CB04200  
Lossman Alan R  
30 8th St  
Astoria, OR 97103

80909CC01300  
Navarro Rachel  
Navarro Jane  
8655 SE Silver Creek Rd  
Happy Valley, OR 97086

80909CC01600  
Neikes Thomas R 1/2  
Neikes James J 1/2  
34755 Hwy 101 Business  
Astoria, OR 97103-6664

80908DA00500  
WH Properties LLC  
337 N Cutter Cir  
Portland, OR 97217-3940

80909CC01100  
O Shay Betty  
2652 Grand Ave  
Astoria, OR 97103-2916

80909CB06835  
Oliver Jerry G  
Oliver Annie E  
2775 Mill Pond Ln  
Astoria, OR 97103

80908DA01500  
Pacific Coast Pharmacies LLC  
Steve M Choate  
124 Westview Pl  
Kalama, WA 98625-9640

80909CB06838  
Parfitt Dennis P  
Parfitt Mary Helen  
2715 Mill Pond  
Astoria, OR 97103

80909CC01400  
Ranta Simo J  
511 Harison Ave  
Astoria, OR 97103

80909CB06836  
Redwine Jeffrey Mark  
Redwine Tommie Jo  
PO Box 447  
Astoria, OR 97103-0447

80909CC00400  
Reyes Timothy  
Ecoff Elizabeth  
2659 Marine Dr  
Astoria, OR 97103-2927

80909CB04700  
Ridout LLC  
860 Beach Dr  
Seaside, OR 97138-5504

80909CC00300  
Rutherford Stanley W/Sharon M  
91122 Fort Clatsop Rd  
Astoria, OR 97103-8563

80909CB06845  
Ryan John O  
Ryan Melanie Rae  
2495 Mill Pond Lane  
Astoria, OR 97103

80909CB06850  
Saar William A  
Jensen-Saar Arlee J  
37751 Highway 30  
Astoria, OR 97103

80909CB06849  
Saar William A/ Arlee J  
736 8th Ave NE #219  
Issaquah, WA 98029

80909CC00701  
Spence Barbara  
Neikes James J  
34755 Hwy 101  
Astoria, OR 97103

80909CB06842  
Storey Family Trust  
Storey Cheryl A Trustee  
5770 SW 163rd Ave  
Beaverton, OR 97007

80909CB06808  
Thompson Russell E/Donna Lee  
265 23rd St  
Astoria, OR 97103-2229

80909CB05000  
Treber Gordon A/Nicole  
2778 Grand Ave  
Astoria, OR 97103-2906

80909CC00800  
Walkowski Allison Kaye  
Smith Grady A M  
2624 Grand Ave  
Astoria, OR 97103

80909CB06837  
Wolcott James A  
Wolcott Lorraine E  
2735 Mill Pond Ln  
Astoria, OR 97103-3305



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371 W Grand Ave  
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Astoria, OR 97103-2906

80909CC00800  
Walkowski Allison Kaye  
Smith Grady A M  
2624 Grand Ave  
Astoria, OR 97103

80909CB06837  
Wolcott James A  
Wolcott Lorraine E  
2735 Mill Pond Ln  
Astoria, OR 97103-3305

# The Daily Astorian

949 Exchange Street, Astoria, OR 97103

## Proof of Ad

04/26/18

Account: 341	Ad ID: 54921
Name: NANCY FERBER	Description: "AB6749 CITY OF
Company: CITY OF ASTORIA	ASTOR"
COMMUNITY DEVELOPMENT	Run Dates: 04/27/18 to 05/03/18
Address: 1095 DUANE ST	Class: 101
ASTORIA, OR 97103	Class Desc: Legal Notices
Telephone: (503) 338-5183	Sales Rep: Danielle Fisher
	Cost: \$100.80
	Words: 201
	Blind Box:

Publications
Daily Astorian, dailyastorian.com
Ad Note:

Please proof your ad and contact your sales representative as soon as possible if there are any changes.

### Questions?

Danielle Fisher  
Email: [dfisher@dailyastorian.com](mailto:dfisher@dailyastorian.com)  
Phone: (503) 325-3211

AUTHORIZATION: Under this agreement rates are subject to change with 30 day notice. In the event of a cancellation before schedule completion, I understand that the rate charged will be based upon for the number of insertions used.

AB6749

### CITY OF ASTORIA NOTICE OF PUBLIC HEARING

The City of Astoria Design Review Committee will hold a public hearing on Thursday, May 3, 2018 at 5:30 p.m., in the Astoria City Hall Council Chambers, 2 nd Floor, 1095 Duane Street, Astoria. The purpose of the hearing is to consider the following request(s):

1. Design Review DR17-03 by Garry Vallaster, Astor Venture, LLC to construct an approximately 11,580 square foot commercial building at 2350 Marine Drive (Map T8N-R9W Section 9CB; portion of Tax Lot 6803; portion of Block 144, Shively's, within the Gateway and Civic Greenway Overlay Zone in the LS (Local Services) Zone.

For information, call or write the Community Development Department, 1095 Duane St., Astoria OR 97103, phone 503-338- 5183.

The location of the hearing is accessible to the handicapped. An interpreter for the hearing impaired may be requested under the terms of ORS 192.630 by contacting the Community Development Department at 503-338- 5183 48 hours prior to the meeting. The Astoria Design Review Committee reserves the right to modify the proposal or to continue the hearing to another date and time. If the hearing is continued, no further public notice will be provided.

Published: April 27th, 2018.

## Nancy Ferber

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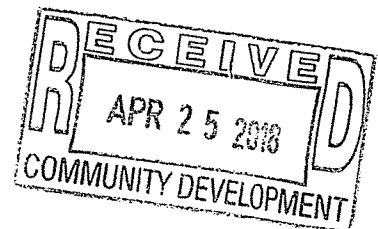
**From:** BARBARA A BOWER <barbbower@msn.com>  
**Sent:** Tuesday, April 24, 2018 3:22 PM  
**To:** Nancy Ferber  
**Cc:** castorey@frontier.com; arlee.jensen@gmail.com; huffmangary@hotmail.com  
**Subject:** Mill Pond Entrance / Egress concerns - seeking 23rd street resolution  
**Attachments:** Co-op entrance and egress concerns - Steam Whistle Way; Mill Pond - Co op Steam Whistle Entrance.docx

Nancy,

Please include this in the packet for the Design Review Committee hearing, for Thursday May 3 @ 5:30 p.m. - re: the Co-op / Astor Venture project.

Thank you.  
Barb

Barb Bower  
2410 Aurora Avenue N.  
Suite.108  
Seattle, WA 98109 U.S.A.  
t. 206.285.3005  
c.206.919.1388  
e. [barbbower@msn.com](mailto:barbbower@msn.com)



April 24, 2018

City of Astoria Design Review Committee

1095 Duane St.

Astoria, OR 97103

Re: Proposed Co-op Entrance and Egress

To whom it may concern:

On behalf of concerned Mill Pond homeowners, we are submitting the following for your review and consideration.

Attached, please find our concerns as expressed in March 2018.

Secondly, upon review of the City of Astoria Traffic Code - it states specifically the intent of being for the ***"Safety of Automobiles, Bicycles and Pedestrians."***

Lastly, in review of the traffic study, as forwarded - we find the following relevant data:

- The traffic study done for rezoning looked at 4 Intersections on Highway 30, but nothing on Steam Whistle Way.
- The traffic study states that the traffic volume for the "Spot Use" will be double the allowed use under the prior zoning. Daily trips will double from 613 to 1227, and the peak hour trips will double from 57 to 114. All of these trips will directly impact Steam Whistle Way.
- Without a traffic study on Steam Whistle Way, the impact of increased traffic of autos, bicycles and pedestrians cannot be assessed.
- However - without a traffic study, common sense suggests that the level of traffic on Steam Whistle Way will be very dangerous and extremely ***"unsafe."***

Thank you for your time and consideration.

Arlee Jensen and Bill Saar      Owners of Mill Pond Lots #17 & #18

Barb Bower and Gary Huffman   Owners of Mill Pond Lots #19 & #20

Cc: Nancy Ferber

Cheryl Storey

Mr. Mike Morgan  
Special Projects  
City of Astoria  
1095 Duane Street  
Astoria, Oregon 97103  
Email - [mmorgan@astoria.or.us](mailto:mmorgan@astoria.or.us)

March 4, 2018

Dear Mike,

We are writing to express our concern with the proposed Steam Whistle Way access to the new Astoria Co-op. We are the owners of Mill Pond Lots 17,18,19 and 20 which are immediately north of the proposed Co-op. Our building sites extend from 23<sup>rd</sup> Street east for 140' and the proposed change to Stream Whistle Way has direct impact on our properties. It is our intent to begin building this year and as I'm sure you are aware these lots require alley load garages. The concept of "alley" vs thoroughfare is a critical element here. Alley load garages are typically built with low traffic alley access to ensure the safety and convenience of residents in accessing and exiting their garages. Steam Whistle Way was designed with this in mind.

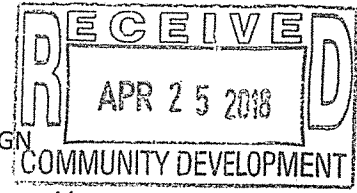
In reviewing the site plan for the proposed Co-op, it is clear that they are proposing all incoming and exiting traffic utilize Steam Whistle Way. This will clearly impact our properties, both in convenience and safety. In addition, we will have ADA requirements that will need to be considered, as this will be the primary egress for one of the owners. It is our belief that the Co-op site plan, with the traffic entrances as currently proposed, will have significant negative impact on the safety and livability of our properties.

It is our request that the Co-op redesign its traffic patterns to enter and exit on 23<sup>rd</sup> Street, as it is already established as commercial access for multiple businesses.

Should you like to discuss this with us in person, we would be happy to meet with you. Our primary point of contact is Barb Bower. She can be reached at 206-285-3005.

Respectfully,

Barbara Bower  
Gary Huffman  
Arlee Jensen  
William Saar



## MILL POND RESIDENTS' RESPONSE TO CO-OP DESIGN

### FOR MAY 3, 2018 CITY OF ASTORIA DESIGN REVIEW COMMITTEE HEARING

**EXECUTIVE SUMMARY:** The Mill Pond residents are supportive of the Co-Op but object to the present Co-Op plan using Steam Whistle Way ("SWW") as access to the Co-Op parking lot. However, we are supportive of Option B as provided by the Developer in the package we received from the City of Astoria. Option B (Exhibit 1, page 3 of 7) sites the back of the Co-Op building on the new divided lot line and access to the Co-Op parking lot is via 23<sup>rd</sup> Avenue. We also would like to see a sound deadening wall around the loading dock bay rather than a mesh fence with plantings.

**BACKGROUND:** The Mill Pond residents that live between 23<sup>rd</sup> and 27<sup>th</sup>, with access to their garages on Steam Whistle Way (SWW), have joined together to object to the current site-plans (both for the "As Submitted" plan and "Response to Neighborhood" plan) for the Astoria Co-Op, where the back of the Co-Op is sited on 23<sup>rd</sup> Avenue and/ entrance to the Co-Op parking lot is via Steam Whistle Way ("SWW"). Further, SWW has a long curve with limited visibility impacting those whose garages are along this street. Numerous Mill Pond residents have voiced concerns over the use of SWW to access the Co-Op parking lot. We are very concerned about the substantial increase in traffic on SWW and access in and out of our garages, resulting in a significant safety issue. The traffic study as presented by the Co-Op does not include any traffic study and impact on SWW nor on Mill Pond Lane. The existing traffic study only focused on Marine Drive and 23<sup>rd</sup> Avenue.

As far as we can ascertain, the re-zoning and division of the lot, formerly known as the Wauna Lot 144, was approved by the City Council after a recommendation for approval by the Planning Commission. At both these meetings, objections to the plan for the Co-Op to use SWW was submitted for consideration. According to Page 6, in a draft of the March 5, 2018 minutes, Councilor Price stated that at least 120 cars a day would need to visit the Co-Op in order for the store to succeed. Jennifer Bunch stated on Page 2 of the minutes that there would be 73 new trips during the peak hour of 4:15pm to 5:15pm. This level of traffic on SWW would be hazardous for the Mill Pond residents having to back in and out of their garages.

Due to our concern with the proposed Co-Op access via SWW, a Mill Pond resident created a revised site plan (Exhibit 2, p 4 of 7), which we find is VERY similar to Option B (page 3 of 7) as prepared by the developer, and both of these plans mitigate our objections to the "As Submitted" plan and the "Response to the Neighborhood" plan. We believe Option B also satisfies the Co-Op's requirements as submitted in their drawings. Please note that the Response to Neighborhood plan as submitted to the Design Review Committee (DRC) to "satisfy" the Mill Pond HOA Board is still not acceptable and was only approved, with objections, by the HOA Board after receipt of a January 25, 2018 e-mail from Don Vallaster threatening to send their request to mediation and binding arbitration (Exhibit 3, p. 5 of 7). The HOA Board always considered SWW access as unacceptable.

Attached is Exhibit 4 (pages 6 and 7) with excerpts from the e-mail string with the HOA Board and developer regarding the existing Co-Op plan.

The Mill Pond residents' want to make clear their objection to the developer's "As Submitted" plan and the developer's "Response to Neighborhood" plan as follows:

1. The use of SWW for access/egress is unacceptable due to the Mill Pond residents' garages having to pull in and back out onto SWW and concern about increased Co-Op traffic flow on SWW. Our concern is also for the safety of the mobility impaired residents in the existing apartments whom are in wheelchairs and scooters using SWW for access to other locales.
2. The back of the Co-Op building is along 23<sup>rd</sup> which is not consistent with a pedestrian friendly view corridor.

3. Traffic generated to/from the Co-Op is concentrated on both 23<sup>rd</sup> Avenue and SWW for access. The move to Option B allows for Co-Op traffic to access the parking lot from 23<sup>rd</sup>, removing SWW as primary access.

A couple further comments are:

4. Landscaping should be consistent with the drawings as shown for both the revised Co-Op Plan as well as Option B.
5. We ask that a concrete sound wall around the loading area be implemented rather than a mesh fence with plantings to reduce noise. This request is for both the currently revised plan as submitted to the City as well as with Option B plan (which is the preferred plan by the Mill Pond residents).

In summary, we wholeheartedly support Option B, which uses 23<sup>rd</sup> Avenue to access the Co-Op parking lot. Both our proposed plan and the developer's Option B plan are pedestrian friendly and visually consistent with the original vision of Mill Pond Village.

Respectfully submitted, Mill Pond Residents:

Arlee Jensen and Bill Saar  
Lots 17 and 18 (corner of 23<sup>rd</sup> and Mill Pond Lane)

Mark and Tommie Redwine (Lot 30)  
2755 Mill Pond Lane

Barbara Bower and Gary Huffman  
Lots 19 and 20 (next to Saar's lots)

Jerry and Annie Oliver (Lot 31)  
2775 Mill Pond Lane

Ben and Sally Jarvis (Lot 21)  
2455 Mill Pond Lane

Toivo and Emilee Fransen (Lot 32)  
2795 Mill Pond Lane

John (MP HOA Secretary) and Melanie Ryan (Lot 22)  
2495 Mill Pond Lane

Mark Waidelich and Jacinta Chvatal (Lots 11, 12,  
and 73)  
2875 Mill Pond Lane

Tracy and Donna Black (Lot 23)  
2505 Mill Pond Lane

Ron and Nancy Sweet (Lot 45)  
2879 Mill Pond Lane

Dick and Sandy Darby (Lot 24)  
2555 Mill Pond Lane

Dan and Shelley Morgado (Lot 39)  
2860 Mill Pond Lane

Harvey and Cheryl (MP HOA President) Storey (Lot 25)  
2605 Mill Pond Lane

Jerry and Colleen (MP HOA Landscape Chair)  
Keenan (Lots 14 and 15)  
260 23<sup>rd</sup> Street

Lois Hankins (Lot 26)  
2655 Mill Pond Lane

Alice Iverson (Lots 1 and 2)  
285 23<sup>rd</sup> Street

Gary Aspmo (Lot 27)  
2705 Mill Pond Lane

Russ and Donna Thompson (Lot 4)  
265 23<sup>rd</sup> Street

Dennis and Mary Parfitt (Lot 28)  
2715 Mill Pond Lane

Brad (MP HOA Architect Chair) and Morgan  
Kobielusz (Lot 7)  
235 23<sup>rd</sup> Street

Jim and Lori Wolcott (Lot 29)  
2735 Mill Pond Lane



STEAM WHISTLE WAY

PROPOSED LANE

STORMWATER DRAINAGE SWALE

LOADING BAY

GROCERY STORE

MAIN ENTRANCE

OUTDOOR SEATING

48 TOTAL PARKING SPACES

PROPOSED LANE

MARINE DRIVE

VCA  
VALLEYCASTLE CORP  
ARCHITECTS

# SITE PLAN - OPTION B

Page 3 of 7

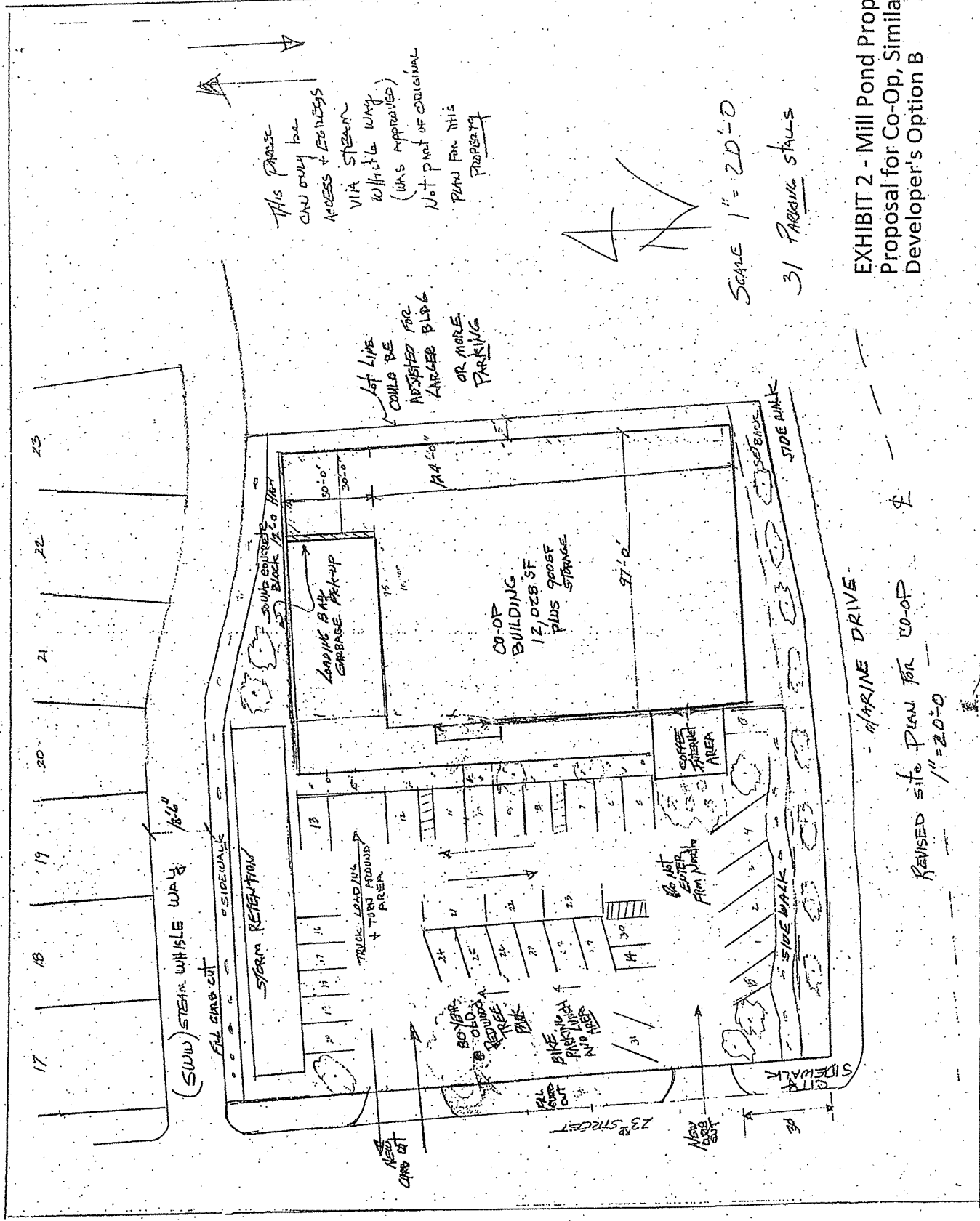


EXHIBIT 2 - Mill Pond Proposed Site  
Proposal for Co-Op, Similar to  
Developer's Option B

### EXHIBIT 3 - Developer's Threat for Binding Arbitration

To the Board of Directors of the Mill Pond Village Homeowners Association

January 25, 2018

I am writing on behalf of Astor Venture, LLC, to request that the full board meet to resolve the denial of our application for architectural review of our submitted design plan for the Wauna Site. We had thought that the Board was going to hear the dispute on January 13<sup>th</sup>, but that scheduled meeting was unilaterally cancelled by the Board. Our appeal is of the determination set forth in the email we received from Cheryl Storey, Board President dated 12-12-17.

At the time we submitted our plans for design review there was nothing in the "Architectural guidelines" that required that access to Lot # 144 be off of 23<sup>rd</sup> street. Indeed those guidelines merely provided for pedestrian access off of Marine drive or 23<sup>rd</sup>, but said nothing about whether Steam Whistle way could or could not be used for purpose of vehicle access. Given that Steam whistle way already allows for vehicle access for vehicles accessing the senior living facility on the adjacent lot, it would appear to be arbitrary to require us to locate our entryway to be on 23<sup>rd</sup>. We have documentation dedicating Steam Whistle Way to the public as a public road. We have met with the City of Astoria and they have approved our use of Whistle way as a means of accessing our lot. We believe the determination to disapprove our design that includes using Steam Whistle way as a means of access to our property, to be arbitrary and not based on guidelines that were in force at time of our application. We have offered to mitigate any impacts on other owners ability to use Steam Whistle way for access purposes by widening Steam Whistle Way, but for whatever reason that entreaty was rejected.

We believe we are in substantial compliance with setback requirements for the rear and side yards adjoining the planned structure. Our plan is for a minimum 5 foot setback on the side yard adjoining 23<sup>rd</sup> street. The rear would be fronting Steam Whistle way and our design plan provides for a minimum set back of ten feet from that street. We are therefore at a loss as to why our set backs are not in conformance with Architectural guidelines and therefore appeal that determination as it may have resulted in denial of our application.

We are willing to revisit the planned use of metal materials for the exterior cladding on the planned structure. That is not a determination we are appealing.

**Please be advised that if the Board determines to not hear the dispute that we are requesting that the matter be submitted to mediation and then binding arbitration, if not sooner resolved, in accordance with section 11.2 of the "Master Declaration of protective covenants, conditions and restrictions for Mill Pond Village. [Emphasis added]** Time is of the essence as we are moving forward with City approvals to bring this project to fruition. Our read of the community is that there is overwhelming support for the COOP and we would hope that to be an impetus to reconsideration of the architectural committee's adverse determination.

Sincerely,

Don Vallaster

Managing Member, Astoria Ventures Partners

## EXHIBIT 4 - E-Mail Excerpts

### Excerpts from E-mail exchange between Mill Pond HOA and Developer

**From:** Cheryl Storey  
**Sent:** Tuesday, February 27, 2018 7:57 AM

Hello, this e-mail is a follow up to the revised plans provided last week, color samples, and exterior material sample. My comments are as follows:

1. The Board approved revised plans as submitted with Steam Whistle widened by 4' the length of the co-op lot, materials and color Cool Zinc Gray, but defer final elevations and exterior building design to the City Planning Committee. *As an aside on the exterior plans, the HOA Board is not comfortable with the rear building design as currently updated, i.e., long wall with extra windows added that may end up partially frosted to hide storage.*

**From:** Cheryl Storey  
**Sent:** Friday, February 9, 2018 7:57 AM

Don, that increase in measurement is fine. Based on your comments below, Steam Whistle Way itself would be widened to 22' with the overall ROW at 23' 6". Yes we understand this does impact the co-op lot size but appreciate the accommodation to ensure better traffic flow for the co-op.

As an aside note, if the co-op building were relocated to the other side of the lot (with the back end of building on the east lot line), then Steam Whistle would not need to be widened as access to the co-op w/b on 23<sup>rd</sup> Avenue. And the co-op overall lot size would not be reduced.

**From:** Don Vallaster [mailto:donv@vcarch.com]  
**Sent:** Thursday, February 8, 2018 10:00 AM  
**To:** Cheryl Storey; et al...  
**Subject:** Re: Astoria Co-Op and Mill Pond HOA Comments

Cheryl,

The ROW is 20' on paper & we propose to widen it to 24'. Since 4' will be new on the south side we can make sure that the top of curb will not encroach unnecessarily on the road width. The COOP will lose up to 4 spaces as a consequence so are a little grumpy. Also their receiving yard will be reduced by 4' to accommodate the wider street.

On 1/28/2018 5:36 PM, Cheryl Storey wrote:

Hello Don, Matt and Jennifer –

... Thank you for meeting with Brad Kobielusz and me on January 6<sup>th</sup> to review the building plans for the proposed co-op development on the Wauna lot. We were to have a follow-up meeting between the Mill Pond BofD and the co-op development the following Saturday, but we needed to cancel this meeting. As I mentioned in my e-mail to you dated January 8<sup>th</sup>, we cancelled this follow-up meeting between the Mill Pond BofD and the Co-Op development to evaluate a few things internally within the Mill Pond HOA before setting up the next joint meeting. We also had additional questions after the January 6<sup>th</sup> meeting that Brad and I asked of you for which you have provided answers.

As Don and I discussed one-on-one on January 19<sup>th</sup> and as discussed again at our Mill Pond Board meetings, we have advocated for the entrance to the co-op to be on 23<sup>rd</sup> Avenue rather than Steam Whistle Way. In order to accomplish this entrance/exit route, the back of the co-op building would be on the East end of the lot line, with the parking lot along 23<sup>rd</sup> Ave. The delivery trucks would then access the loading dock via the 23<sup>rd</sup> entrance into the parking lot as well.

Therefore, our requests/comments are based on 1) the current siting of the building as shown in the drawings provided to the Mill Pond HOA, 2) responses to our follow-up questions after our Jan 6<sup>th</sup> meeting with the co-op

development, 3) additional internal discussions regarding the Co-Op development, and 4) the July 24<sup>th</sup> public meeting. The Mill Pond Board met on January 13<sup>th</sup> and again January 26<sup>th</sup> in order to arrive at the requests/comments outlined below, some of which have already been acknowledged by both the Co-Op and the Mill Pond HOA:

1. Steam Whistle Way is currently 18' wide. The street should be widened to 24' from 23<sup>rd</sup> Avenue to the 2<sup>nd</sup> entrance/exit to the co-op as shown in the drawings. The remainder of Steam Whistle Way would be converted to a one-way street, with the direction heading west.
4. The back of the co-op building along 23<sup>rd</sup> Avenue is mostly metal, with few windows with a cold/harsh look, even with the landscaping. The back should be softened to be more neighborhood friendly. We recommend more windows, such as clerestory, and a mixture of other trims, i.e., wood to break up the back of the building. The Market of Choice Buildings, such as the new one in Bend and another one in Beaverton, are an example. We are not specifying the exact building changes in this memo but look forward to seeing an updated drawing of the proposed changes.

We are working with the COOP on windows on the backside of the building and have agreed to (2) high windows in the receiving room & a large clerestory window over the exit door from the sales area. The COOP will be storing inventory on shelves so a row of continuous windows is not practical. We are looking at softening the metal yard enclosure with a steal grill mounted on off sets to allow vines to cover the wall.

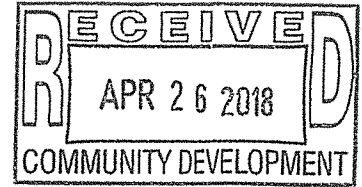
8. We ask for the use of sound deadening material be lined on the interior of the docking bay walls to mitigate noise from the delivery/garbage trucks, as the docking bay is on the North side of the co-op building, closest to the single-family homes across Steam Whistle Way.
10. In response to our questions, the following was provided by the co-op:
  - a. The co-op gets most deliveries in the morning and agrees to no deliveries before 6AM. The largest size truck is a 65' tractor trailer.
  - b. The loading dock/garbage and recycling areas will have fenced enclosures that will be locked when no staff are present. *MP HOA NOTE: this is to avoid transient issues, such as sleeping overnight in these areas and the location used as drug drops.*
  - c. The co-op does not have any plans for self-service bottle return machines and will be handling bottle returns inside the store, similar to how it's done currently.
  - d. Expected co-op hours at the new location may be from 7am to 9pm. Staff are usually at the store an hour or so before opening and an hour after closing.
  - e. The co-op does not expect daily deliveries to increase much from that being received currently.

Other comments outside the construction plans:

12. The Mill Pond HOA believes traffic will be a far worse issue than has been provided in the traffic study. Don, whom owns the gas station at the corner of 23<sup>rd</sup> and Marine Drive, was exactly correct in his impartial comments on Jan 24<sup>th</sup> in how it is 8 months of the year. We live in this community and the best way to get around during the busy months is by walking in lieu of driving.

Should there be any significant changes to the building design and location (i.e., siting), we reserve the right to modify and update our responses.

Thank you, Mill Pond HOA Board of Directors



Design Review Committee  
City of Astoria

April 26, 2018

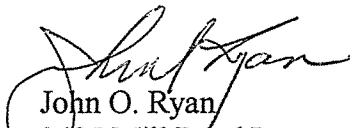
Re: Design Review DR 17-03

Shopping at the CO-OP and living with the CO-OP

The addition of the Astoria CO-OP to the Mill Pond area is a life changing event. For those of us that live adjacent to Steam Whistle Way we will have a traffic situation that will adversely effect our lives forever. For those who live in Mill Pond the added traffic will change the character of a quite ,unique to Astoria ,pedestrian orientated residential area. It said that adding traffic to a residential area is the best way to ruin the quality of life for the homeowners.

I must decide , attached (A walk to the Park #1) is my written picture of life with the CO-OP as submitted and life with a relocated CO-OP to the east. I have offer an optional design (attached #2) that meets the relocation criteria.

This is a life changing decision being considered by the Design Review Committee. Looks like the best I can expect is the building sited to the east property line and the entrance and exit to parking via 23<sup>rd</sup> street. This is also to the benefit to the city of Astoria.  
Thank You.

  
John O. Ryan  
2495 Mill Pond Lane  
Astoria, Oregon 97103

Attached: A Walk to  
the Park #1  
Revised Site  
Plan #2

## A Walk to the Park

Downtown doing some shopping, I decide on such a nice day to go to a park I have heard about and never had a chance to visit, Pergola Park at De Muro Mill Pond Village. Driving east on Marine Drive, about 22<sup>nd</sup> street and the CMH building the traffic starts to slow and I wonder what is happening. Anyway it gives me a chance to get my bearings and see that 23<sup>rd</sup> street, just past the Shell station, is just ahead. I know the park is on 23<sup>rd</sup> street and one block to the north. I notice that there is not a left turn arrow or left turn lane on Marine drive and the traffic turning off and on 23<sup>rd</sup> is a real mess. I wonder if it is caused from the metal building along 23<sup>rd</sup> Street. I do see a small outdoor dining area close to the road and figure this must be part of the Astoria Co-op building. Guess all this traffic and street parking, wherever there is room, is from the Co-op. What about their parking lot and entrance, must be down 23<sup>rd</sup> where there is a truck, large semi, trying to back up for a delivery. 23<sup>rd</sup> Street is a mess of traffic, so I decide to park in the CMH lot south of Marine Drive and the Co-op. Wow, glad to get out of that mess and on foot.

When I get to the "cross walk" at Marine and 23<sup>rd</sup> the traffic is going both ways on Marine, cars entering from 23<sup>rd</sup> are trying to turn left (across traffic) and turning right and at the Shell station cars exiting the station are turning right and left.?? I get the traffic to stop and hurry across Marine as not to interfere with this competitive sport of intersection derby by vehicles from 5 positions. Where is the traffic light to make sense of this, at least the developer should be required to set funds aside to finance this light at a later date (this has happen before). Need to see what is next.

As I start down 23<sup>rd</sup> on the east side, I pass the sidewalk to the outdoor seating area of the Co-op and notice that the parking area is beyond and in front of the building which I cannot see from my vantage point on the sidewalk OR, could I be approaching the entrance drive to the parking area with a sidewalk along side to access the eating area in front of the Co-op that is now located on the east side of the property. Traffic seems to do better when off the street with a parking lot they can see from 23<sup>rd</sup> Street. I seem to remember a memo from the last city manager, Benoit, stating the entrance to this commercial property would be off 23<sup>rd</sup> Street, what happened to that.

I keep walking along the metal building with a small landscape area between the sidewalk and building. What happened to the set back requirement for commercial/residential buildings along streets, the building is long, big like a WALL, and not very inviting as I walk along. OR, walking along a nicely landscaped area with a dominate redwood tree nicely fitting into the park like landscape. I stop to admire the age and grandeur of this tree, imagining what this tree has endured during the uses of this property (saw mill, plywood mill and remediation program). I am amazed when I look across the street and notice the trees twin although not as big and how the developers of the gas station property was able to save this beautiful tree. Might call them the twin towers of 23<sup>rd</sup> street. Enjoying the trees and landscape and to the east a very nicely designed parking lot with easy access to the Co-op building. It is a warm and inviting design and is accessible to everyone on Marine and 23<sup>rd</sup>. The entry to the Co-op is now visible and the customers eating and having coffee would be in sight.

As I walk along, I am amazed at this truck coming out of a loading area directly into the sidewalk and street. A big semi would or could block the sidewalk and street. OR, this could be exit/entrance to the parking lot. Vehicles would be back away from the sidewalk with plenty of clear view of pedestrians and traffic. Any backup would be in the parking lot. The loading dock would be far away from the sidewalk and street. Drivers visibility would be greatly enhanced. Moving along I come to a small

narrow street.

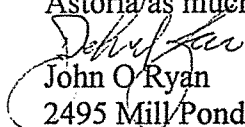
I stop in amazement noticing that just east of the loading dock area is the entrance and exit to the parking lot. What were they thinking, the traffic is backed up on 23<sup>rd</sup> and the traffic trying to enter 23<sup>rd</sup> from Steam Whistle is backed up because 23<sup>rd</sup> traffic can't enter Marine drive, Then I see the traffic leaving the parking lot trying to go west on Steam Whistle Way is being stopped by someone backing out of their garage. Did someone really think this was going to work. Also it looks like a garage door is open and someone probably located in this traffic mess wants to get in their garage. Bet this was a quiet narrow alley before all this happened. I guess they will never figure it out, the traffic report says at least the 2 cars more per minute, might even be worse. It is also obvious the development of the vacant land to east will only have an egress and exit onto Steam Whistle way, more traffic added. Glad no delivery trucks are here today. OR, traffic is controlled on the Co-op property parking area and not public streets (23<sup>rd</sup> Street and Steam Whistle Way) and blocking pedestrian use of public sidewalk.

Finally I get the traffic in and out on Steam Whistle to stop so I can safely cross the ally into the residential area.. Life threatening . Pleasant walk along 23<sup>rd</sup> with a house to my right nicely built and painted to enhance the character of the other houses I can see. How does a large dark metal building against the sidewalk fit into the theme of the Mill Pond area, I ponder.

I get to the corner where across the street is the park. The 23<sup>rd</sup> and Mill Pond Lane intersection and the park should not look like this, Traffic from the Co-op and elsewhere is trying to get to the Co-op or leave the Co-op, not able to use southbound 23<sup>rd</sup>, the cars are turning left or right down Commercial and Millpond Lane and then off to the other connecting street . This is also a mess, wonder what it was like before this. Bet this was a quit unique residential area with just local traffic and a few "looky-loos" moving about. Lots of people walking with or without pets enjoying the pond and the scenery around the homes, not a busy street resembling a downtown traffic day.

Wow again. finally after waiting I get across the street on a painted crosswalk, Cars stop, in a hurry to join the traffic backup ahead. This was worth the walk and the trip through time , present and future (Co-op location along 23<sup>rd</sup> or an alternate location at the east of the property).

Now what is the vision of the City Council and the Design Review Committee for this area , a complete change to the livability of Mill Pond or will it be a decision to mitigate the direct impacts by no zoning change or relocating the Co-op to the west and try to salvage the pristine community in Astoria as much as possible (with no zoning change no Co-op impacts).

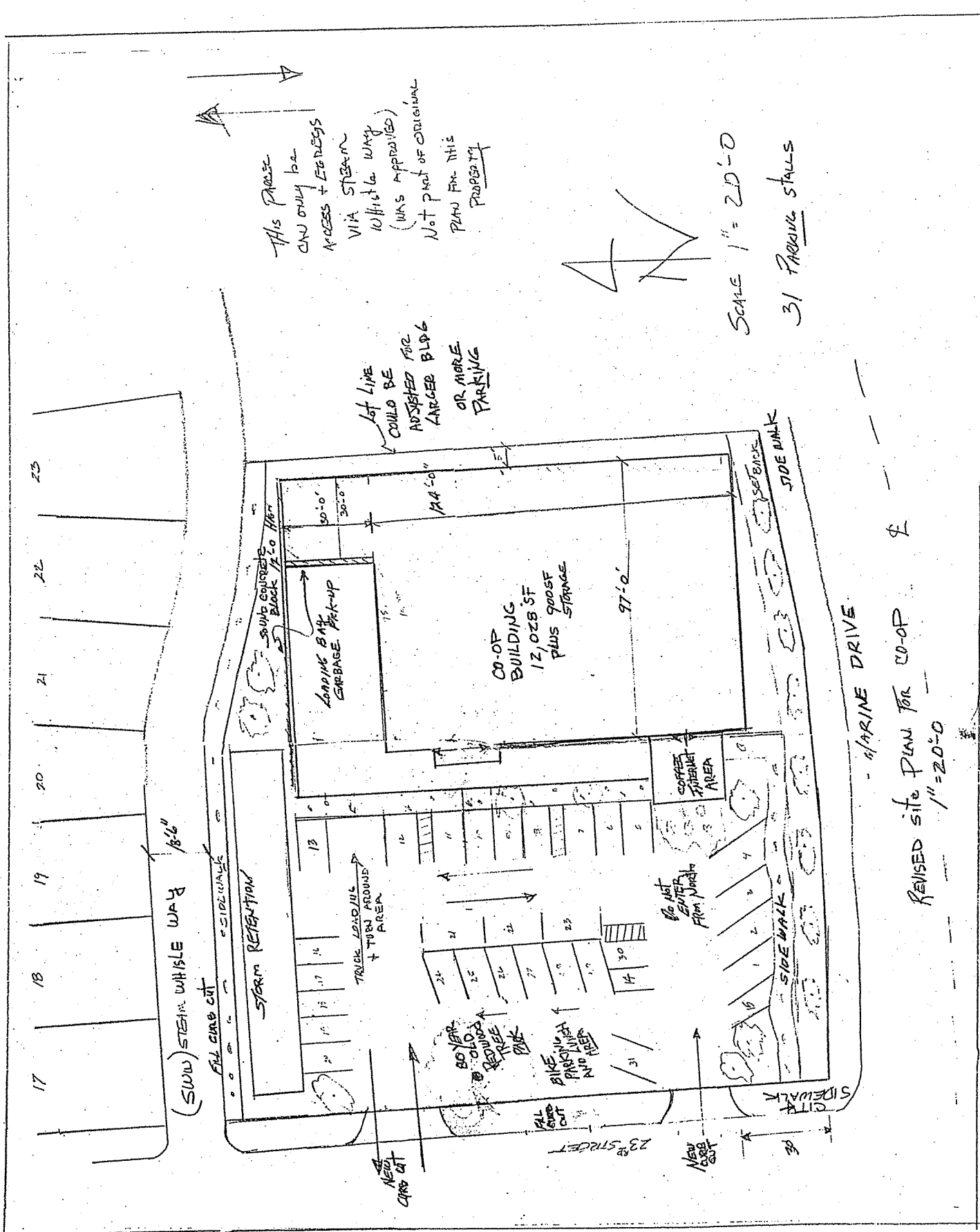
  
John O'Ryan

2495 Mill Pond Lane

Astoria, Oregon 97103

(I am the one trying to get out of the garage)





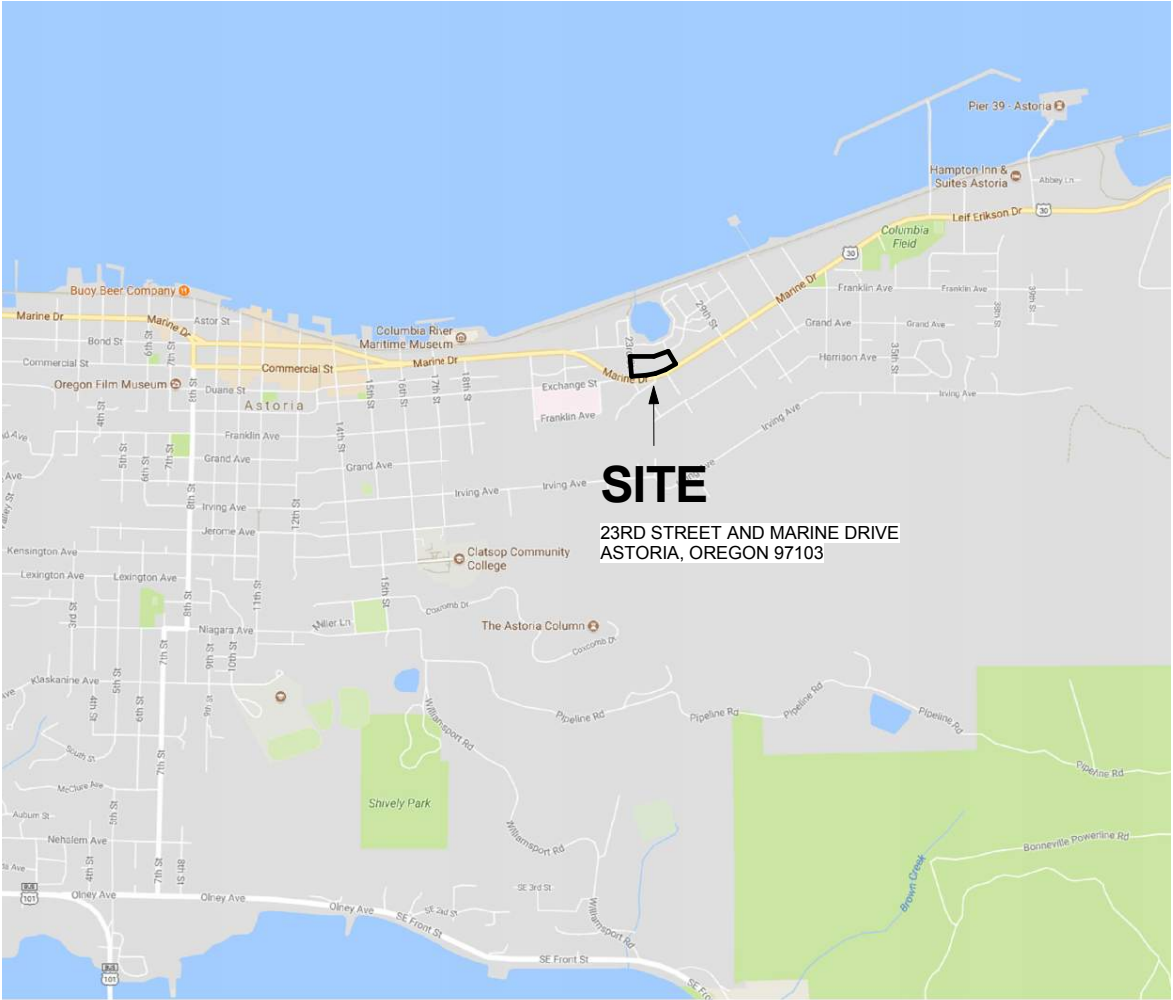






C O N T E N T S

DESIGN REVIEW APPLICATION | PROJECT NARRATIVE



VICINITY MAP

ASTORIA CO OP  
DESIGN REVIEW SUBMITTAL  
APRIL 24TH, 2018

VCA  
VALLASTER CORL  
ARCHITECTS

PLANS AND GRAPHICS

- PREVIOUSLY  
PRESENTED DOCUMENT
- DR1 : COVER
  - DR2 : VICINITY MAP AND INDEX
  - DR3 : CIVIL SITE PLAN
  - DR4 : LANDSCAPE PLAN
  - DR5 : SITE PLAN
  - DR6 : STORE LAYOUT
  - DR7 : ROOF PLAN
  - DR8 : ARCHITECTURAL ELEVATIONS
  - DR9 : ARCHITECTURAL ELEVATIONS
  - DR10 : BUILDING SECTIONS
  - DR11 : BUILDING SECTIONS
  - DR12 : LINE OF SIGHT DIAGRAM

- RESPONSE TO  
HOA + PLANNING
- DR13 : PRECEDENT
  - DR14 : SITE PLAN
  - DR15 : STORE LAYOUT
  - DR16 : ARCHITECTURAL ELEVATIONS
  - DR17 : ARCHITECTURAL ELEVATIONS
  - DR18 : EXTERIOR MATERIALS AND FINISHES
  - DR19 : LANDSCAPE PLAN
  - DR20 : LANDSCAPE PLAN

PLANS AND GRAPHICS

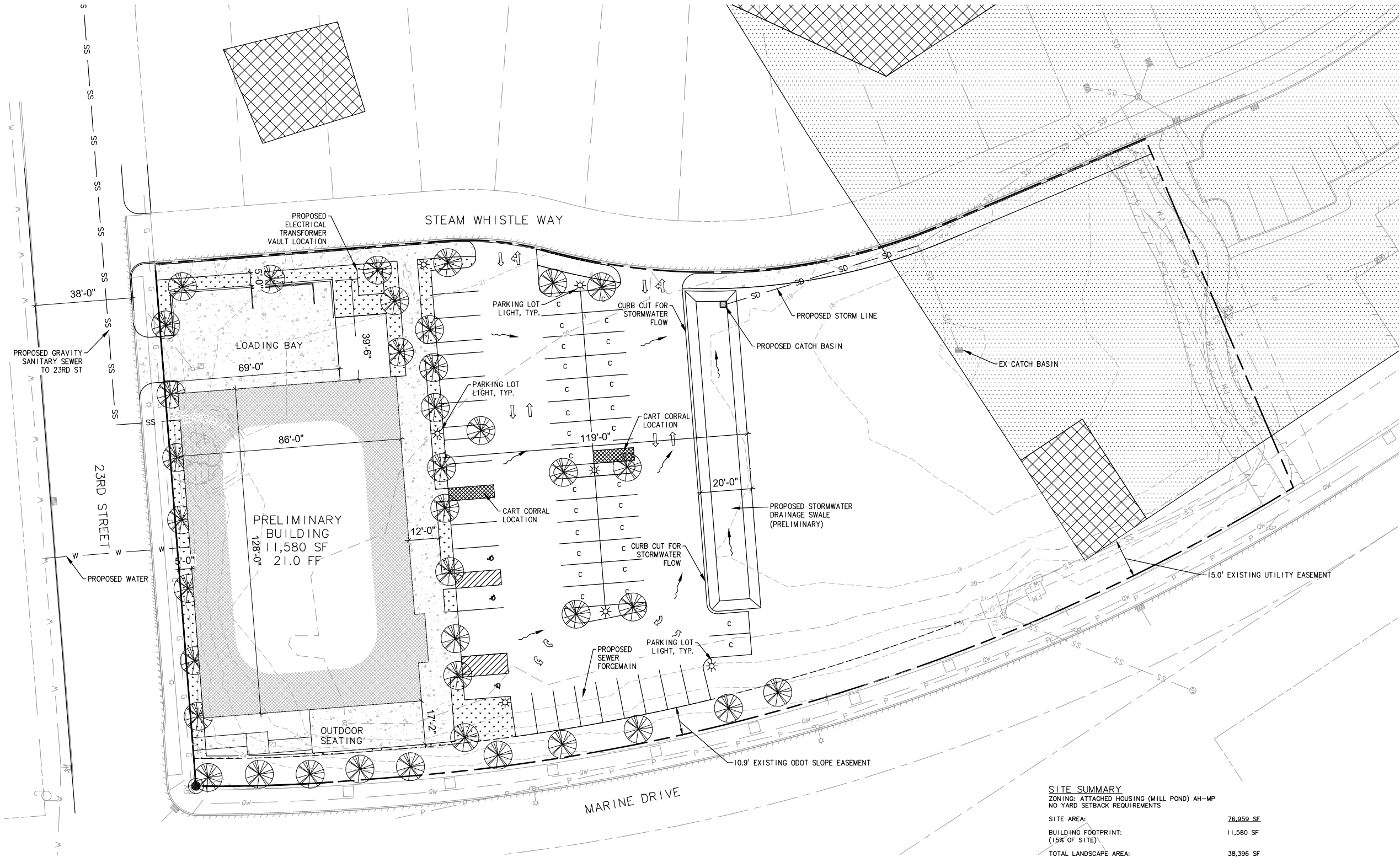
- RESPONSE TO  
HOA + PLANNING
- DR21 : PLANTING SCHEDULE
  - DR22 : NE LANDSCAPE FENCE AXONOMETRIC
  - DR23 : NE LANDSCAPE FENCE AXONOMETRIC
  - DR24 : NW LANDSCAPE FENCE AXONOMETRIC
  - DR25 : NW BUILDING AXONOMETRIC
  - DR26 : SE BUILDING AXONOMETRIC
  - DR27 : PERSPECTIVES
  - DR28 : PERSPECTIVES

- RESPONSE TO  
GATEWAY OVERLAY GUIDELINES
- DR29 : GATEWAY OVERLAY GUIDELINES
  - DR30 : GATEWAY OVERLAY GUIDELINES
  - DR31 : GATEWAY OVERLAY GUIDELINES
  - DR32 : GATEWAY OVERLAY GUIDELINES
  - DR33 : GATEWAY OVERLAY GUIDELINES
  - DR34 : GATEWAY OVERLAY GUIDELINES
  - DR35 : GATEWAY OVERLAY GUIDELINES
  - DR36 : GATEWAY OVERLAY GUIDELINES
  - DR37 : GATEWAY OVERLAY GUIDELINES
  - DR38 : GATEWAY OVERLAY GUIDELINES
  - DR39 : GATEWAY OVERLAY GUIDELINES
  - DR40 : IMAGE



INTERIOR VIEW OF ENTRANCE

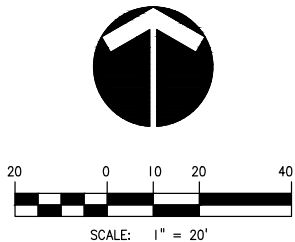
N:\Project\68000\68072\CADD\ACAD\68072T001.dwg Plotted: Aug 14, 2017 -- 1:57pm By: sarahmc



PRELIMINARY SITE EXHIBIT  
SCALE: 1" = 20'

- SOIL CAP AREA AS DETERMINED BY D.E.Q. AND RECORDED IN THE PPA ON FEB. 12, 1999, BOOK 1000 PAGES 700-726
- NO EXCAVATION AREA AS DETERMINED BY D.E.Q. AND RECORDED IN THE PPA ON FEB. 12, 1999, BOOK 1000 PAGES 700-726

<b>SITE SUMMARY</b>	
ZONING: ATTACHED HOUSING (MILL POND) AH-MP NO YARD SETBACK REQUIREMENTS	
SITE AREA:	76,959 SF
BUILDING FOOTPRINT: (15% OF SITE)	11,580 SF
TOTAL LANDSCAPE AREA: (10% REQUIRED -- 50% SHOWN)	38,396 SF
HARD SURFACE AREA: (35% OF SITE)	26,983 SF
<b>PARKING CALCULATIONS</b>	
TOTAL VEHICLE PARKING REQUIRED (INCLUDED 1 ADA STALLS W/1 VAN & LOADING)	24 STALLS
11,580 SF / 500 SF	
TOTAL VEHICLE PARKING PROVIDED (INCLUDED 3 ADA STALLS W/1 VAN & LOADING) FULL 9.5 X 20 (% OF STALLS) COMPACT 8.5 X 16 (% OF STALLS)	51 STALLS
BIKE PARKING REQUIRED 24 VEHICLE STALLS / 10 50% SHORT TERM 50% LONG TERM	3 BIKE STALLS



PRELIMINARY

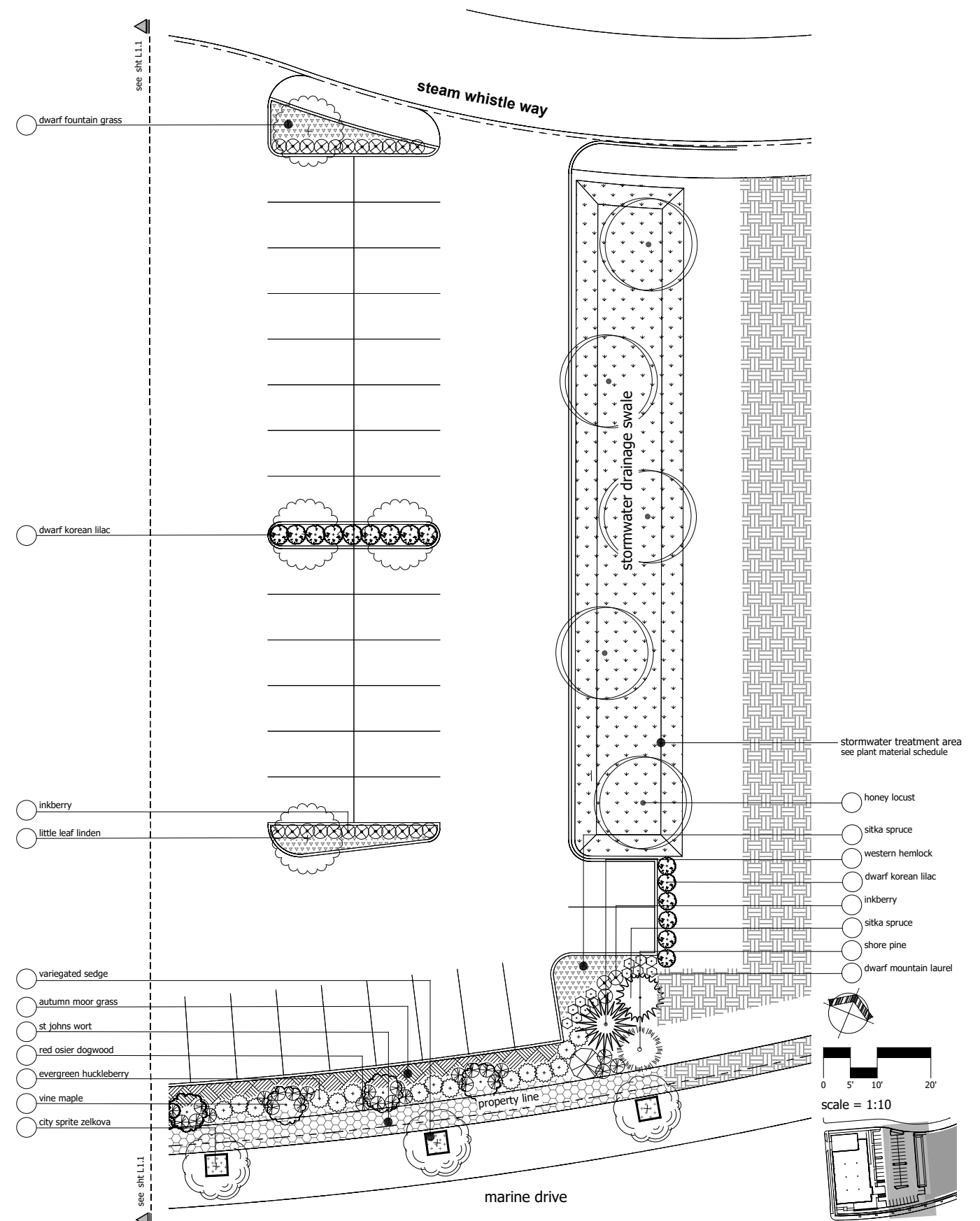
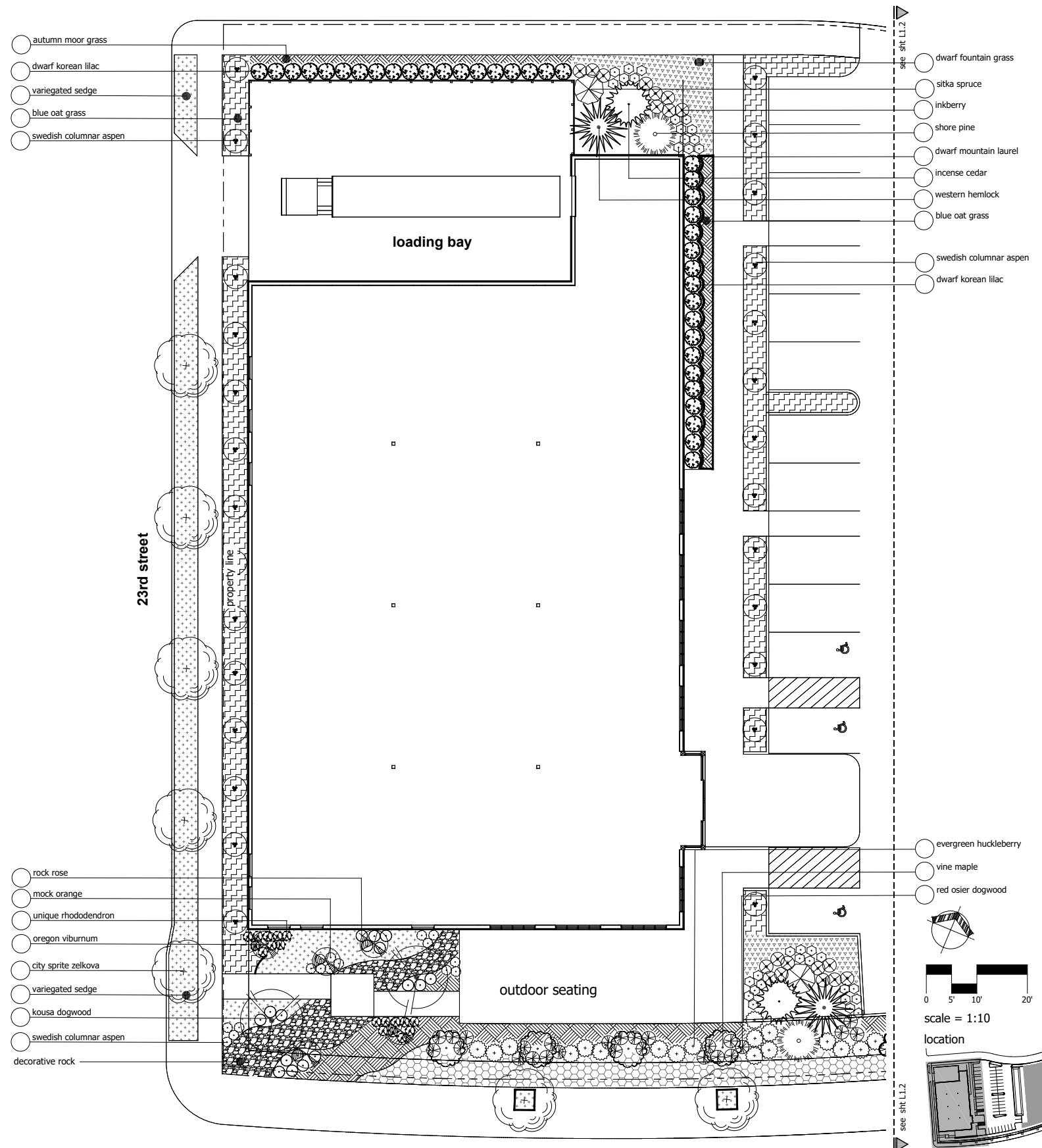
ASTORIA CO-OP  
PRELIMINARY SITE EXHIBIT

ASTORIA, OR



4253-A HWY 101 N.  
GRARHART, OREGON 97138  
Phone: (503) 738-3425  
FAX: (503) 738-7455  
Internet: WWW.OTAK.COM  
68072 C68072T001.DWG  
Project No. Drawing No.  
EX1  
Sheet No.  
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STAMP

PROJECT

ASTORIA CO OP  
ASTORIA, OREGON

CLIENT

PHASE

SCHEMATIC DESIGN

REVISIONS

DATE

AUGUST 8, 2017

PROJECT NUMBER

17.01.01

SCALE

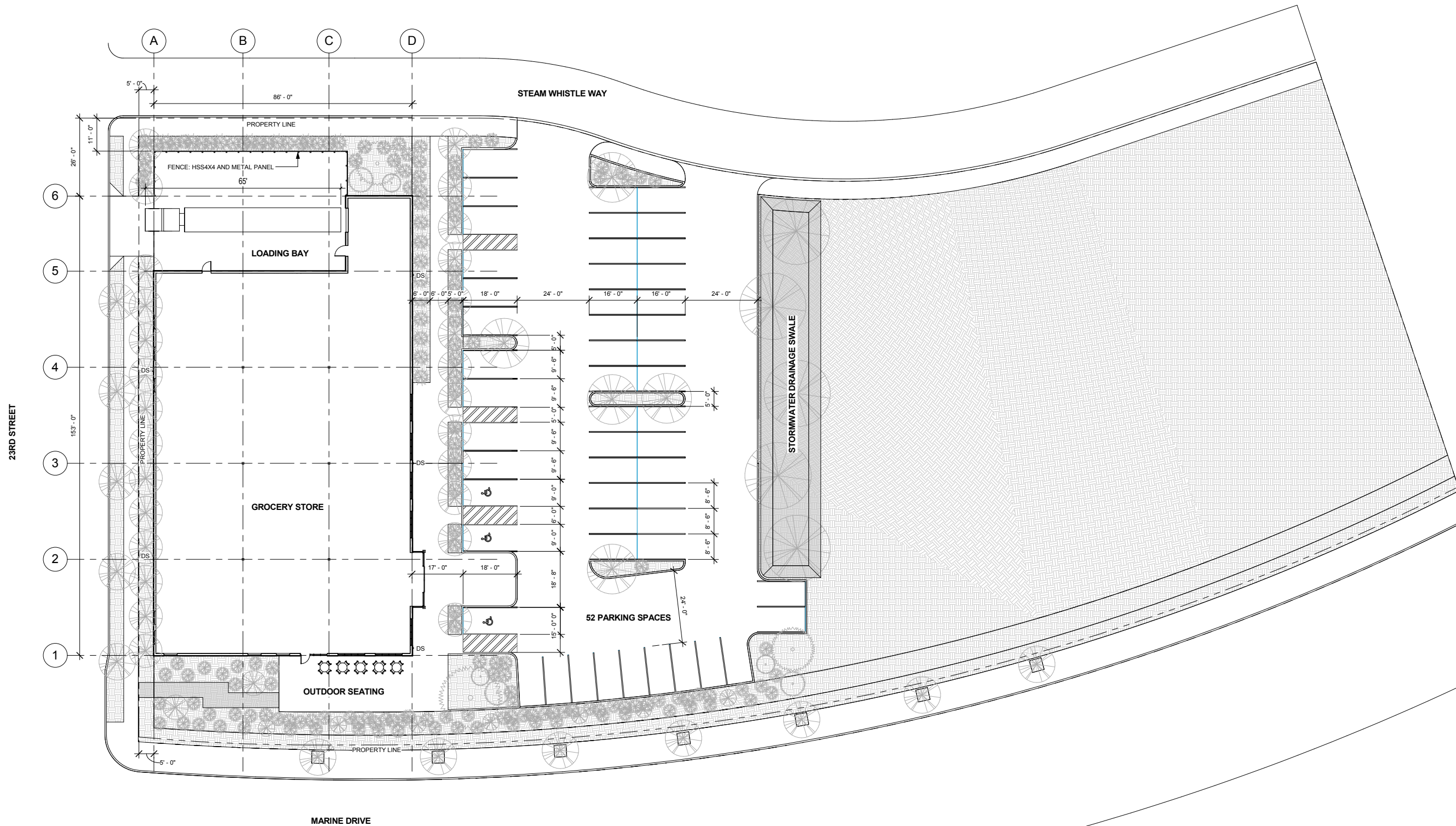
1/16" = 1'-0"

SHEET TITLE

SITE PLAN

**A201**

**5**



A201 | **1** | SITE PLAN  
SCALE: 1/16" = 1'-0"

FULL-SIZE: 24"X36" HALF-SIZE: 12"X18"



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ASTORIA CO OP  
ASTORIA, OREGON

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REVISIONS

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SCALE

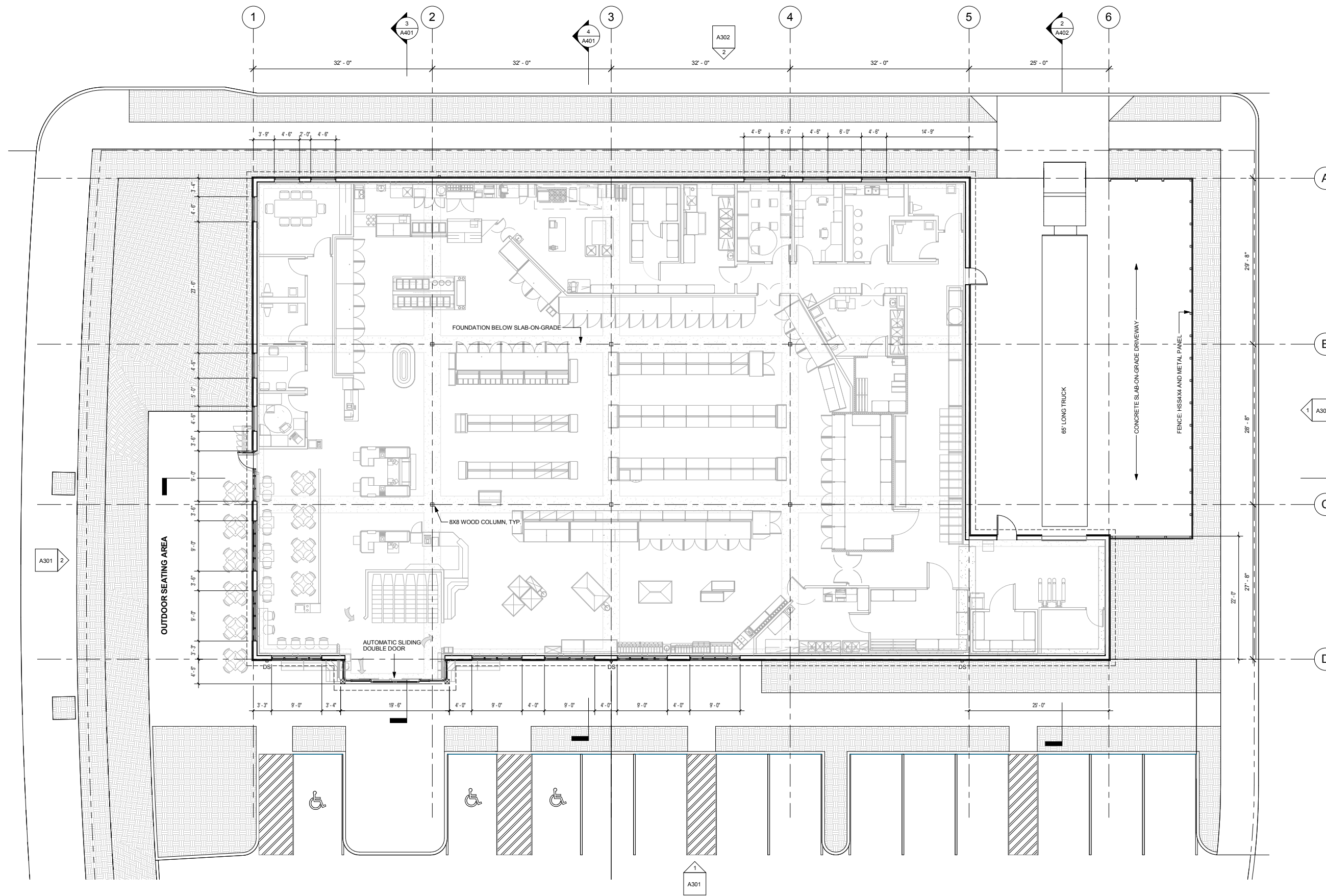
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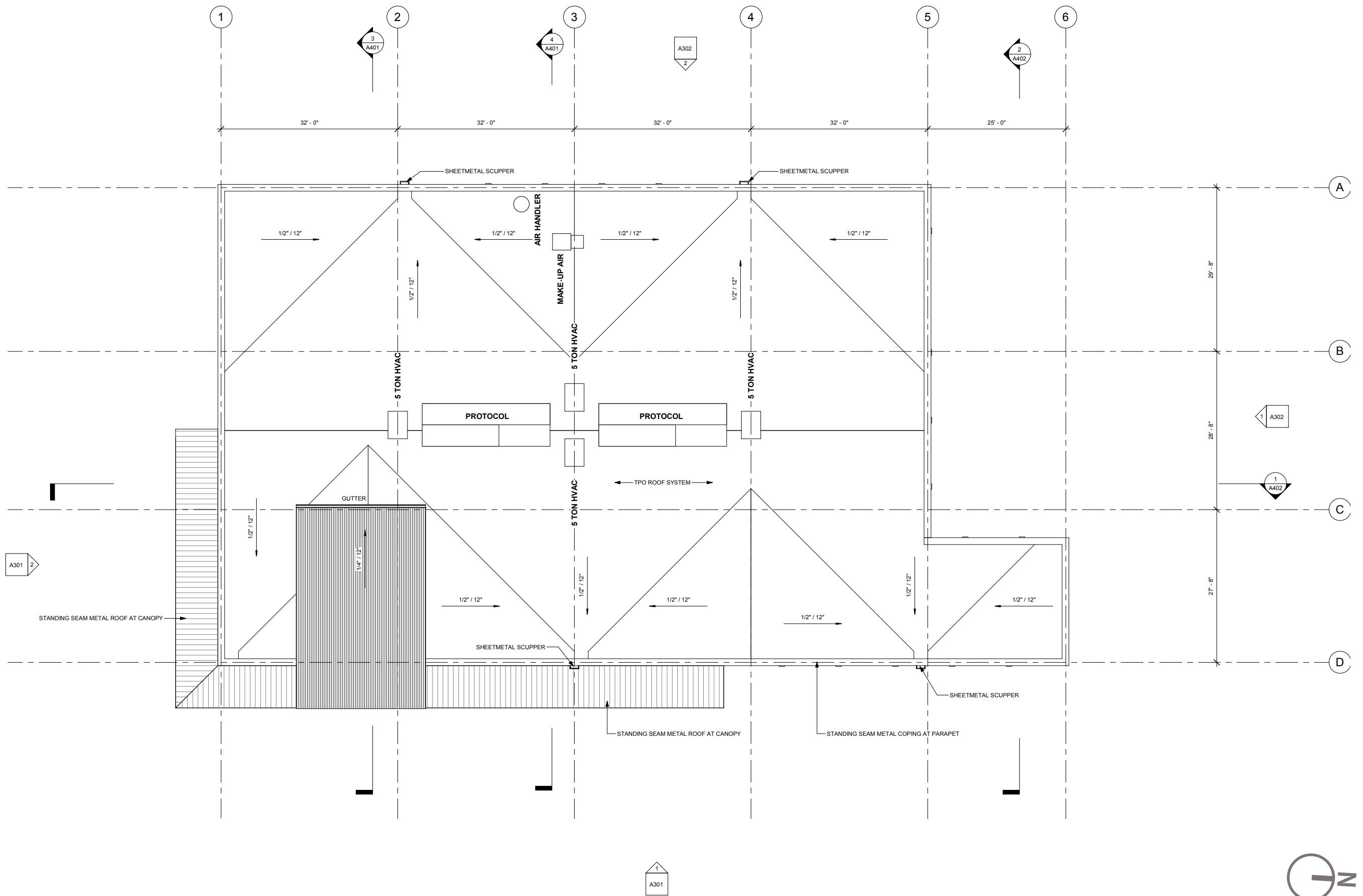
FLOOR PLAN

**A202**

**6**



**GROUND FLOOR**  
SCALE: 1/8" = 1'-0"





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ASTORIA, OREGON

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REVISIONS

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SCALE

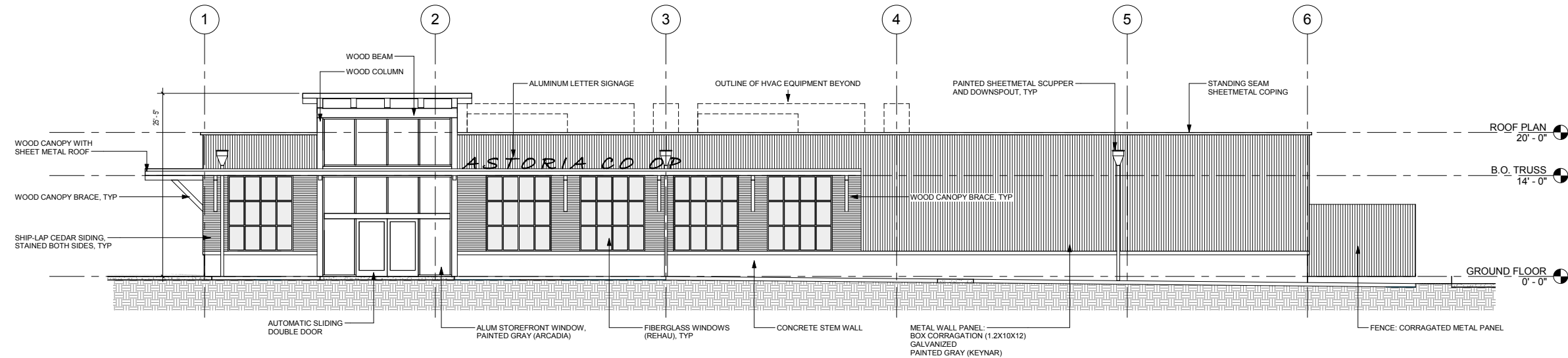
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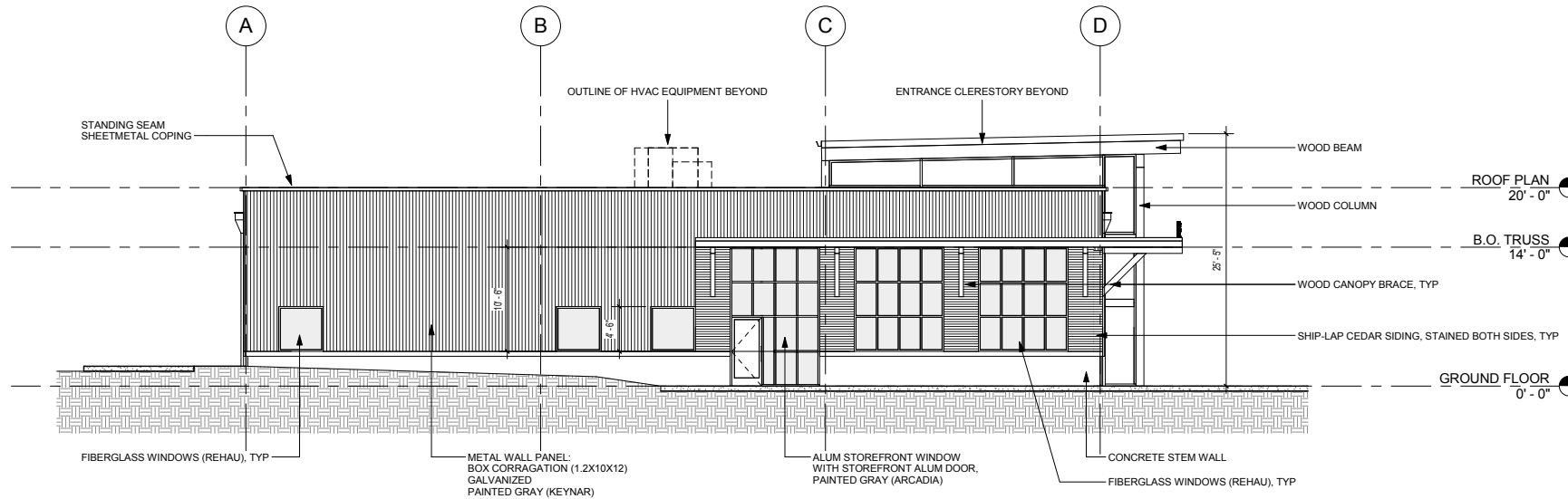
ELEVATIONS

A301

8



A301  
1 EAST ELEVATION  
SCALE: 1/8" = 1'-0"



A301  
2 SOUTH ELEVATION  
SCALE: 1/8" = 1'-0"

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ASTORIA, OREGON

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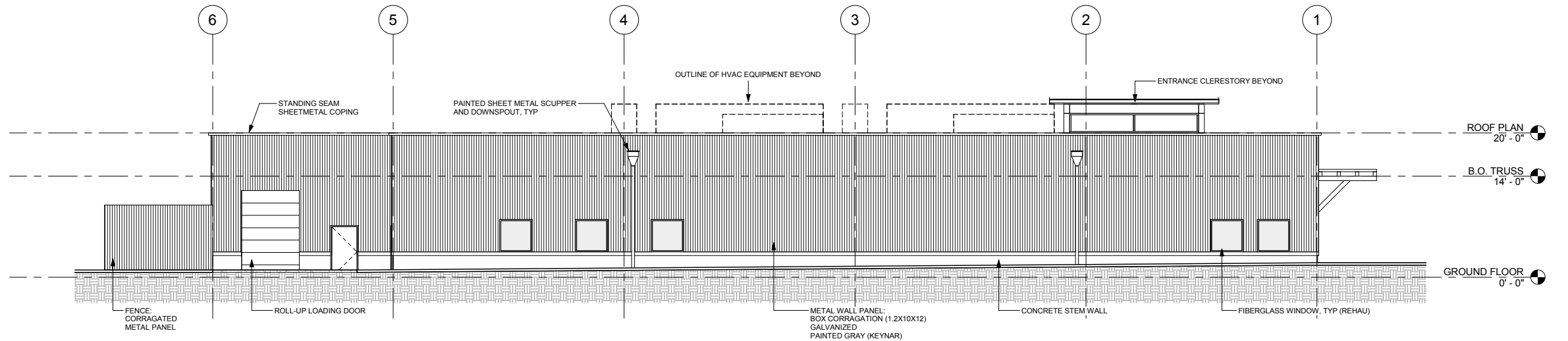
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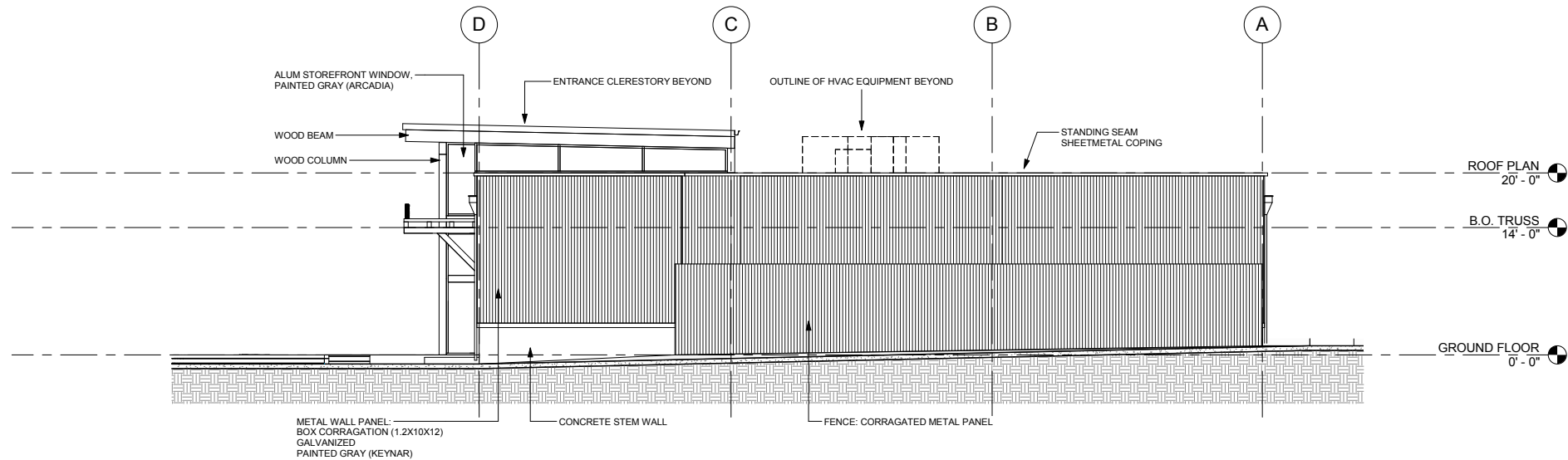
ELEVATIONS

A302

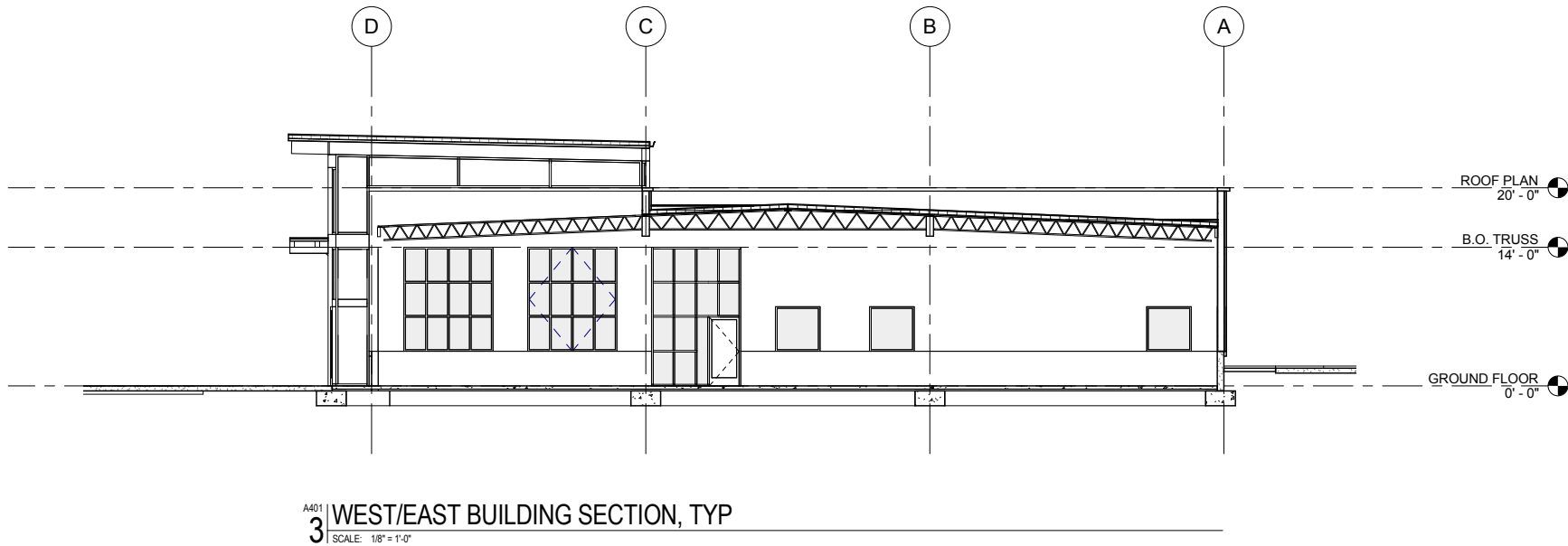
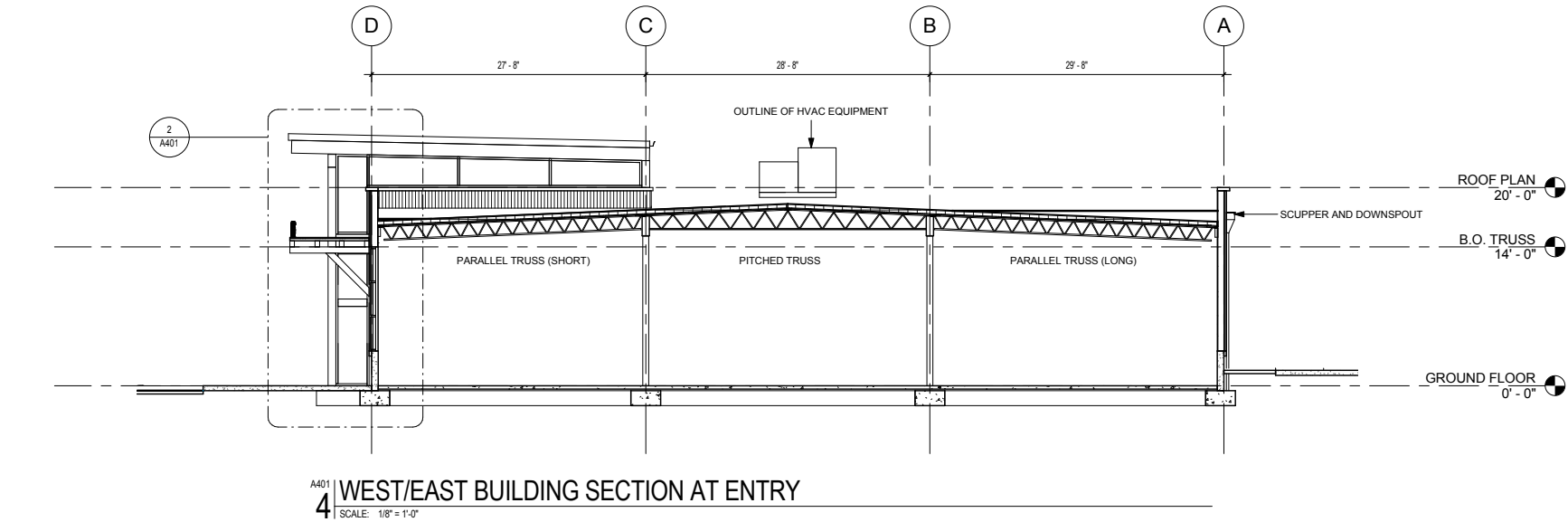
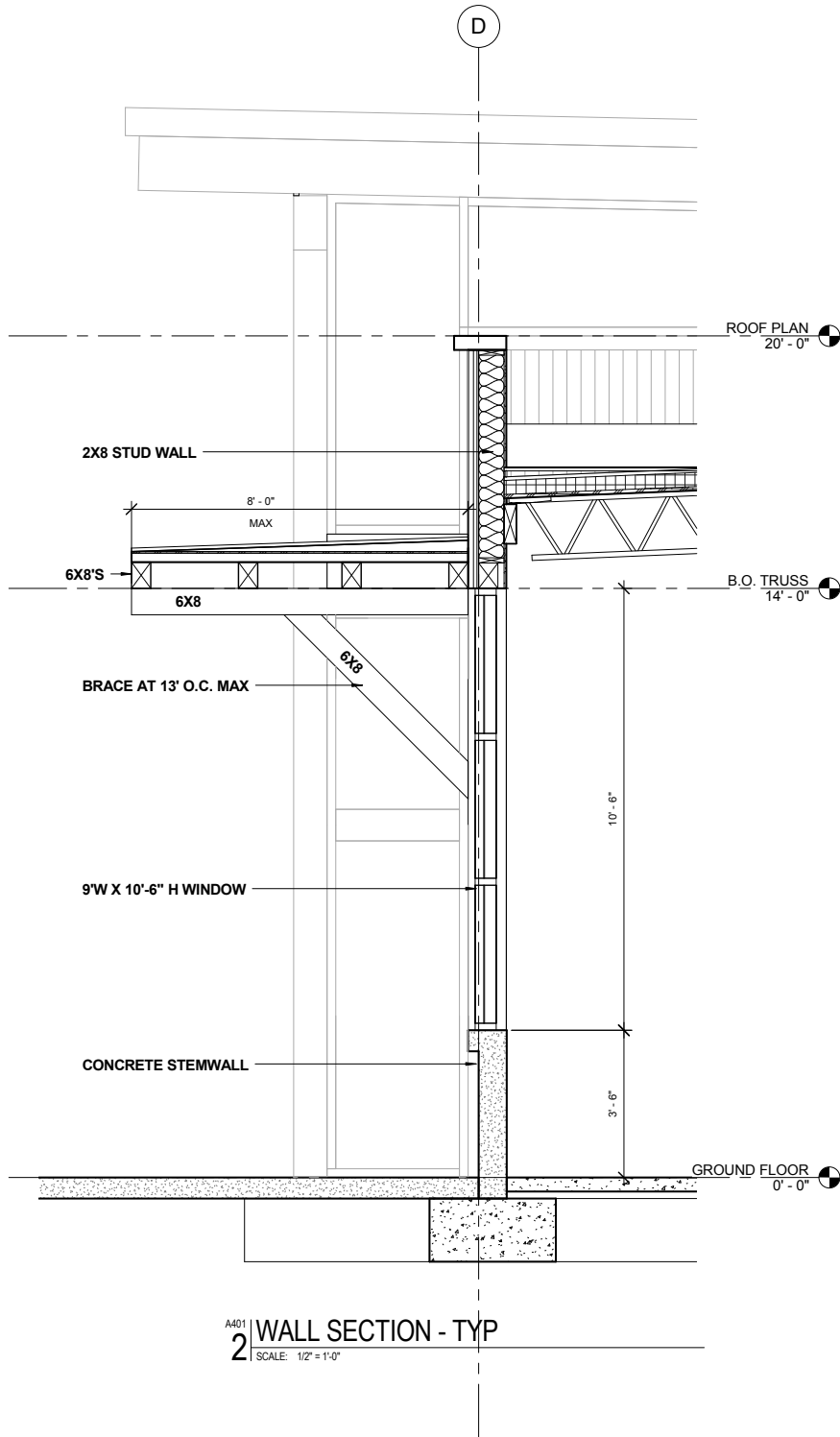
9



A302  
2 WEST ELEVATION  
SCALE: 1/8" = 1'-0"



A302  
1 NORTH ELEVATION  
SCALE: 1/8" = 1'-0"



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ASTORIA, OREGON

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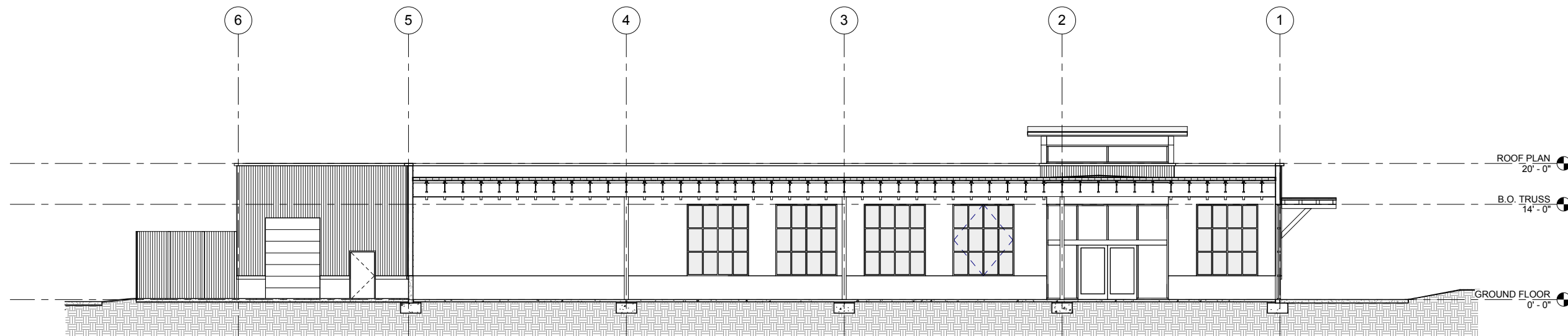
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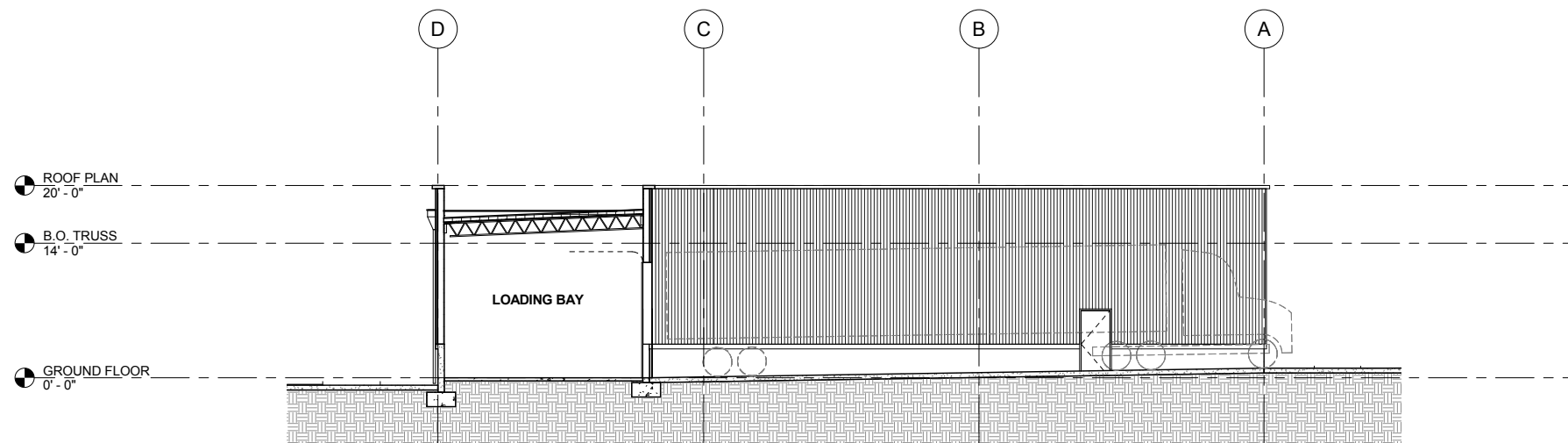
BUILDING SECTIONS

A402

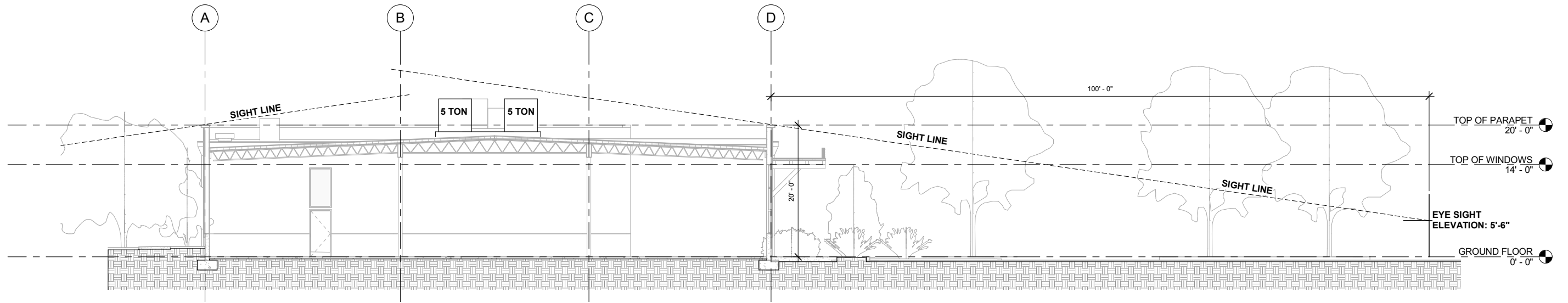
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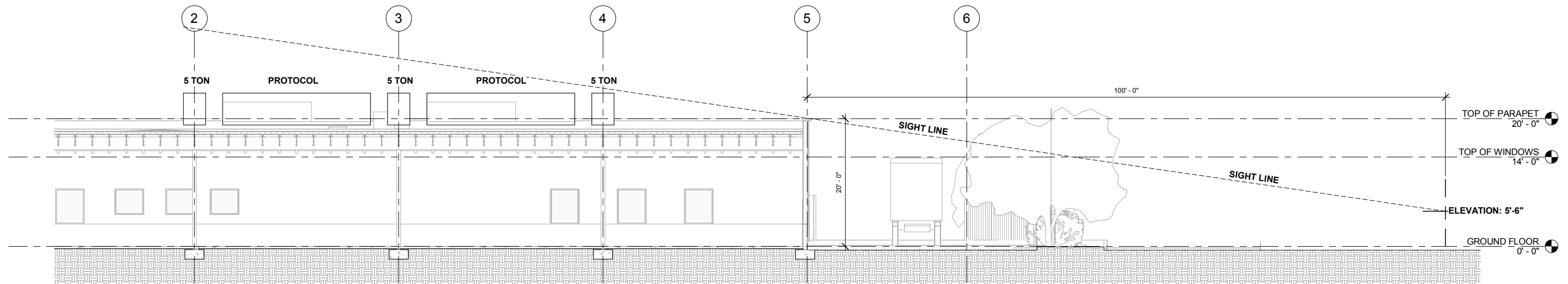
A402  
1 NORTH/SOUTH WALL SECTION  
SCALE: 1/8" = 1'-0"



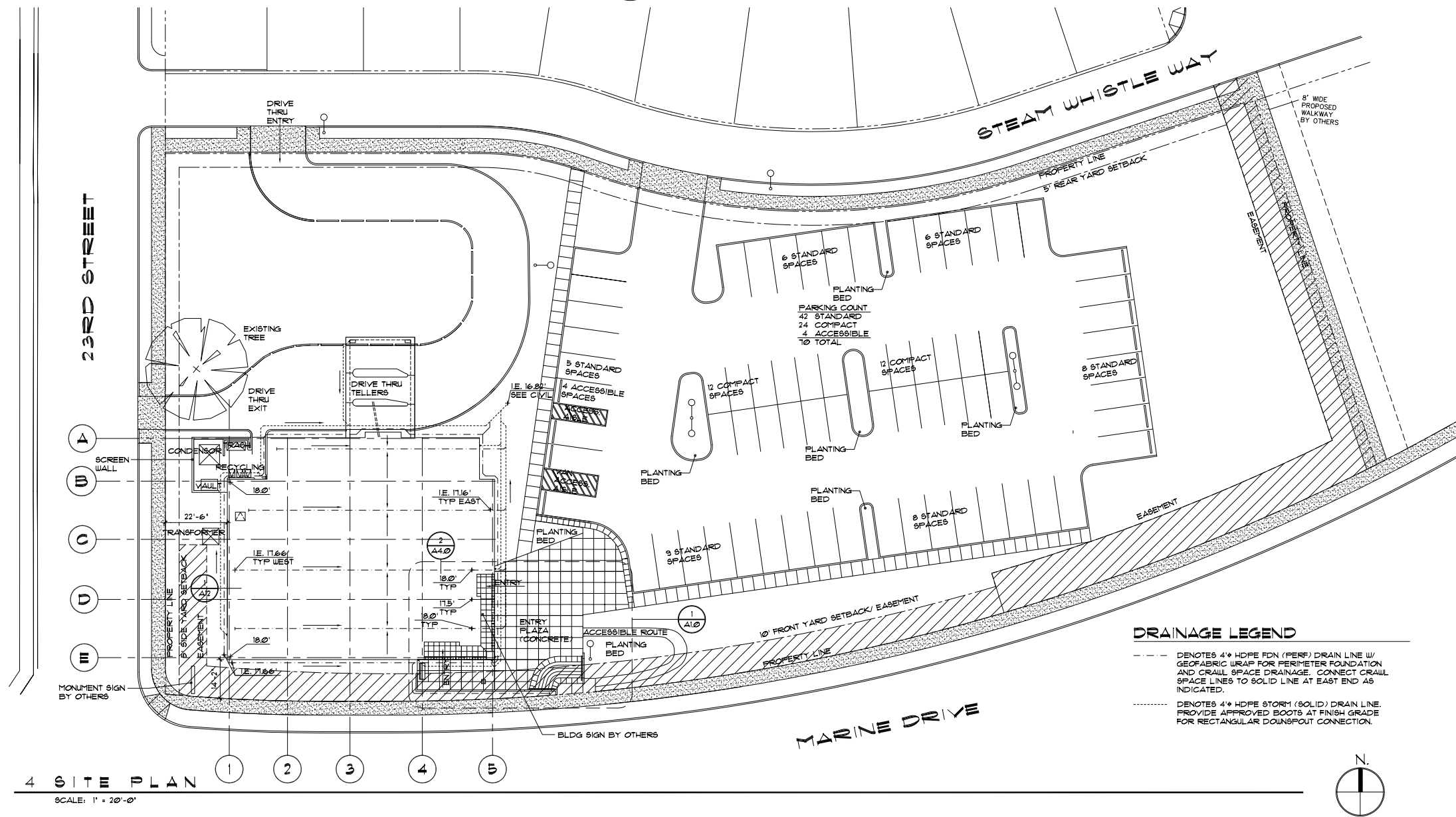
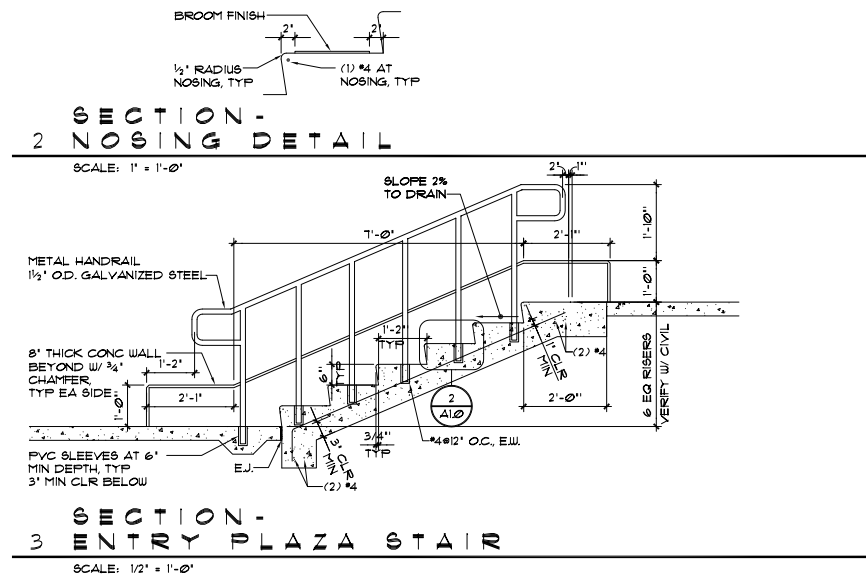
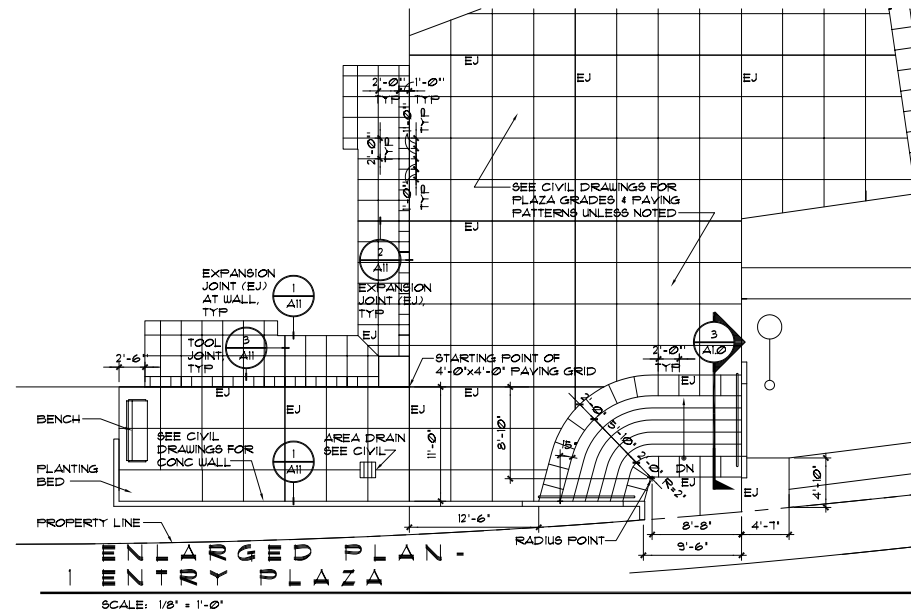
A402  
2 WEST/EAST BUILDING SECTION AT LOADING DOCK  
SCALE: 1/8" = 1'-0"



SIGHT LINES FROM EAST SIDE OF BUILDING  
SCALE: 1/16" = 1'-0"



SIGHT LINES FROM NORTH SIDE OF BUILDING  
SCALE: 1/16" = 1'-0"



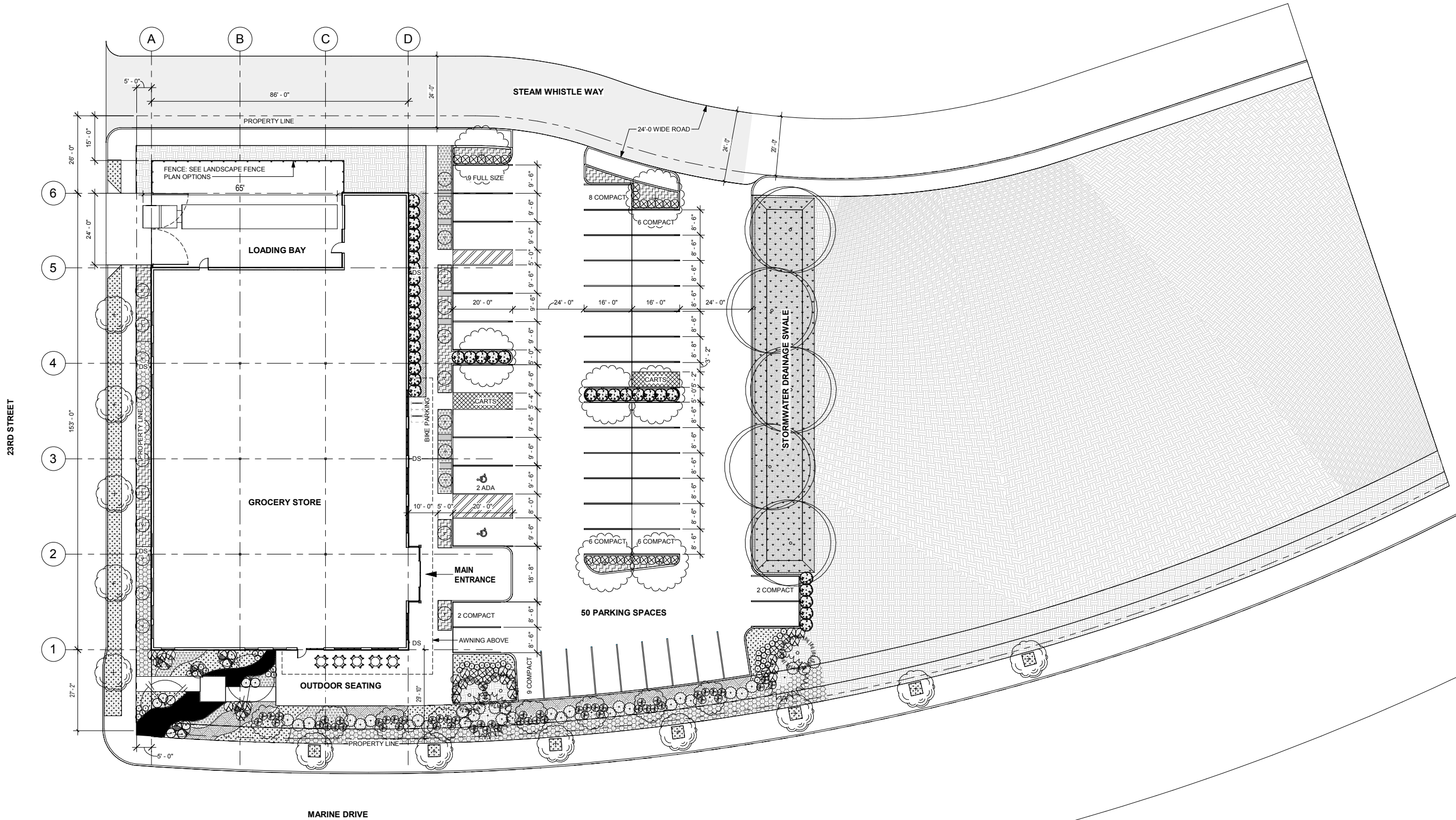
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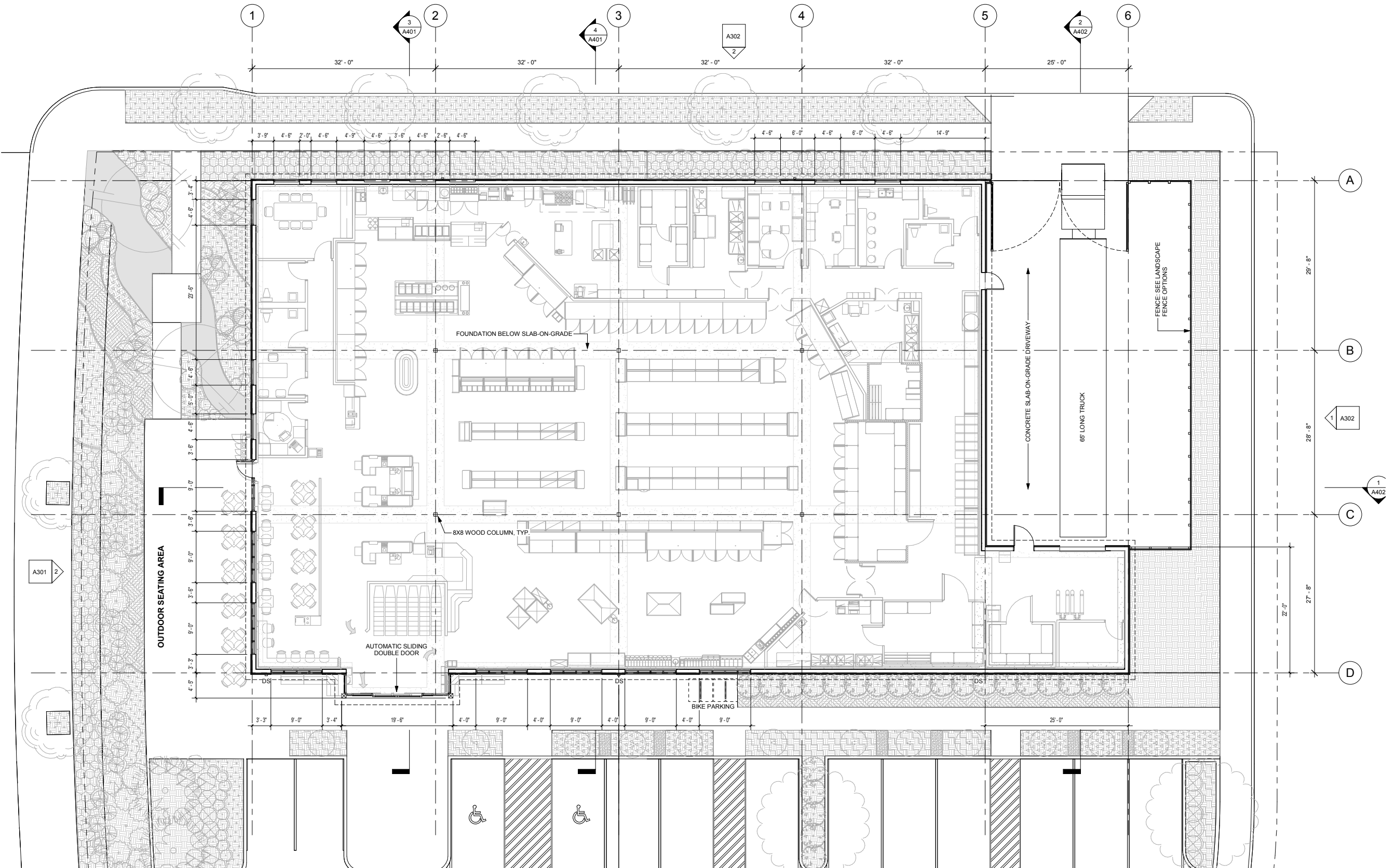
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File #: 9801A1.0-SITE  
Date: © July 1, 2000  
Revisions:

SITE PLAN  
ENLARGED PLAZA PLAN  
PLAZA STAIR SECTION

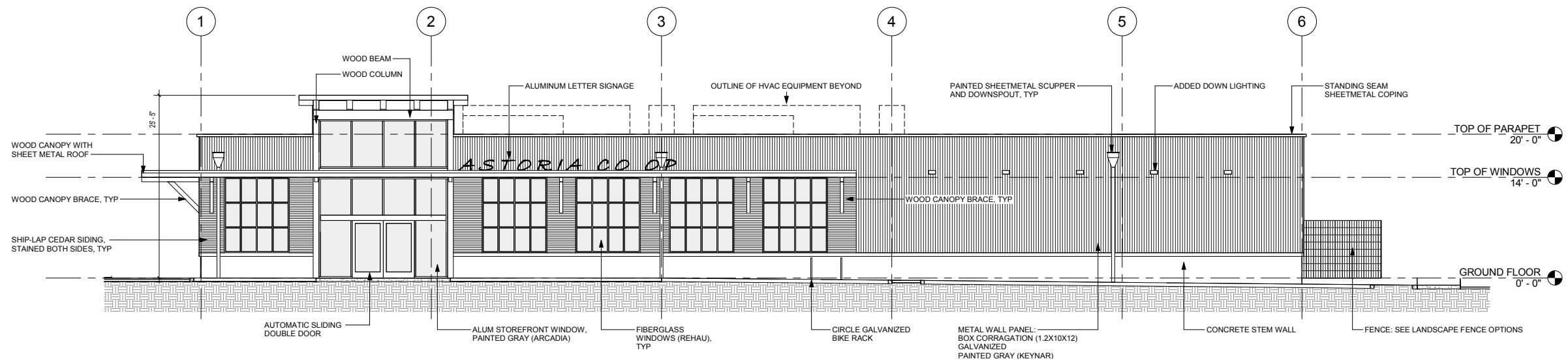
CONST  
A1.0  
SET



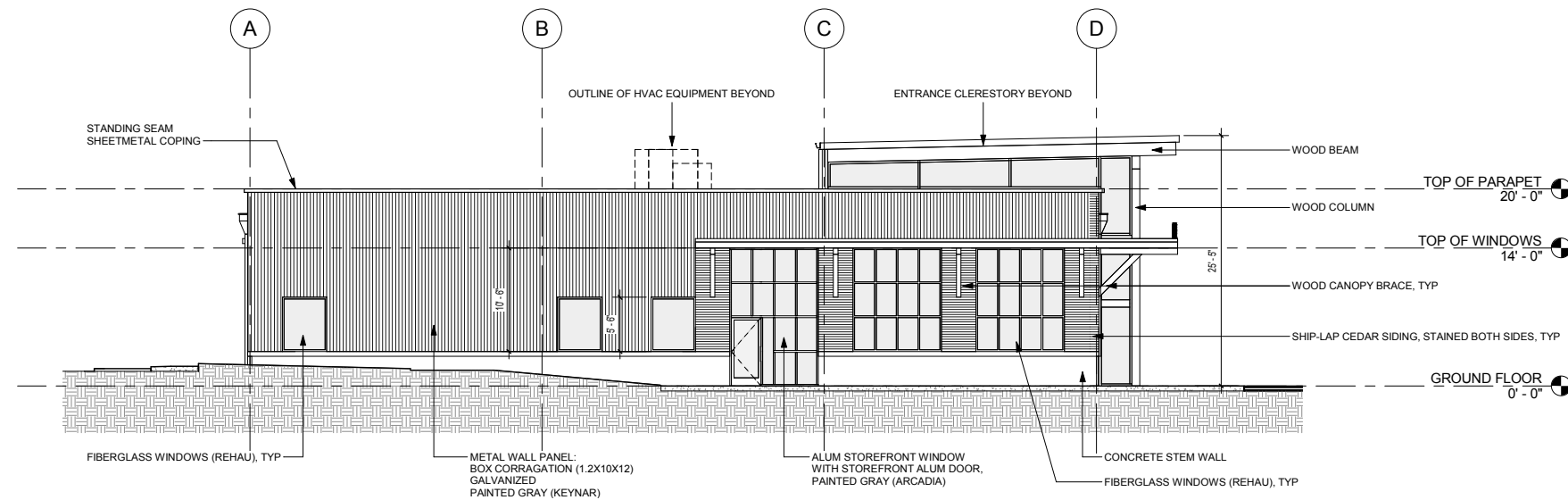




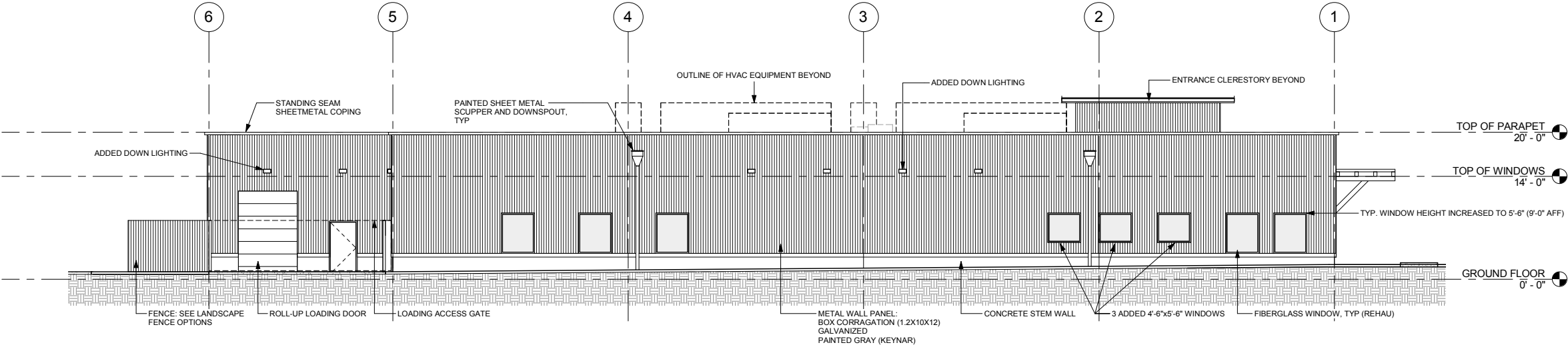




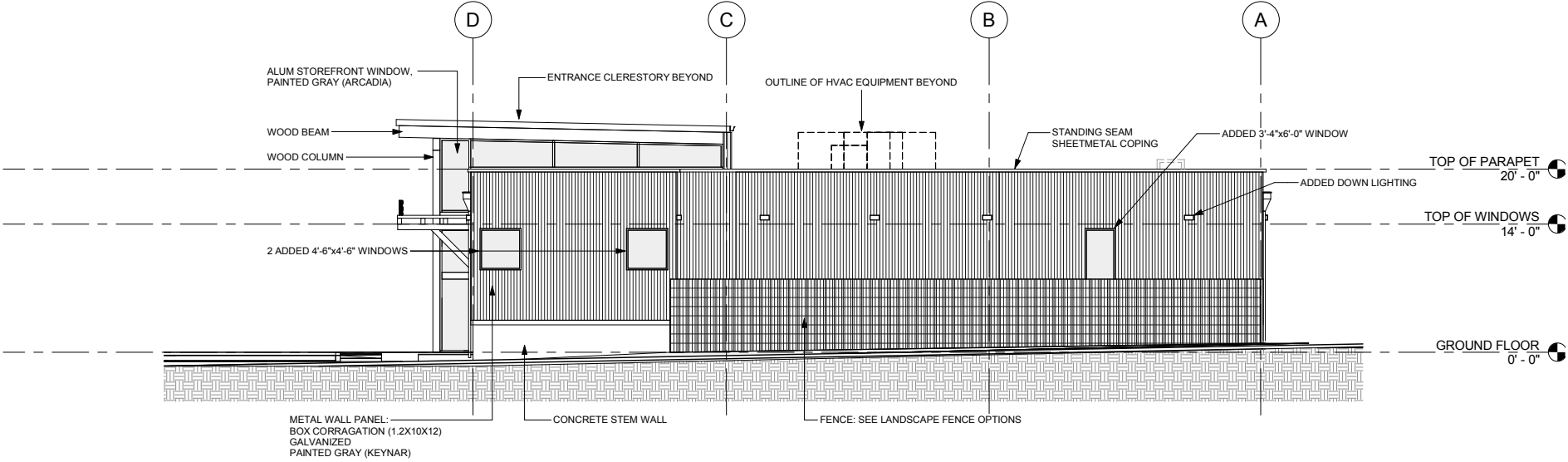
EAST ELEVATION  
SCALE: 1/16" = 1'-0"



SOUTH ELEVATION  
SCALE: 1/16" = 1'-0"

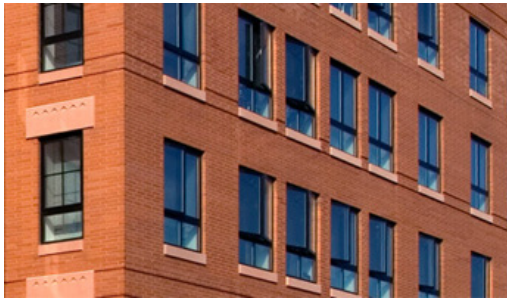


WEST ELEVATION  
SCALE: 1/16" = 1'-0"



NORTH ELEVATION  
SCALE: 1/16" = 1'-0"

CORRUGATED METAL SIDING - VERTICAL RIDGE  
Size: 2" VERTICAL PICKETS  
Color: COOL ZINC GRAY  
Finish: AEP SPAN  
Location: BUILDING SKIN



VINYL WINDOWS  
Size: VARIES  
Finish: WHITE  
Location: ALL WINDOWS

WOOD SHIP-LAP SIDING  
Size: 8" HORIZONTAL BOARDS  
Color: NATURAL  
Finish: CLEAR  
Location: SOUTH AND EAST ELEVATIONS



ALUMINUM STOREFRONT  
Color: ALUMINUM  
Finish: MATTE  
Location: EAST ELEVATION ENTRANCE

WOOD COLUMNS AND BEAMS  
Size: VARIES  
Color: NATURAL  
Finish: CLEAR  
Location: SOUTH AND EAST ELEVATIONS



METAL ROLL-UP LOADING DOOR  
Color: COOL ZINC GRAY  
Finish: CLEAR  
Location: NORTH ELEVATION

CONCRETE STEM WALL  
Size: NOT MORE THAN 4'-6" HIGH  
AT LOWEST POINT  
Color: NATURAL  
Finish: SMOOTH  
Location: EXTERIOR WALL BASE OTHER  
THAN ENTRANCE

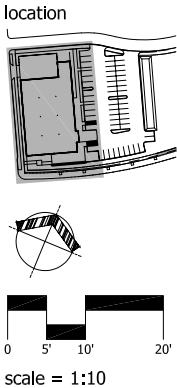
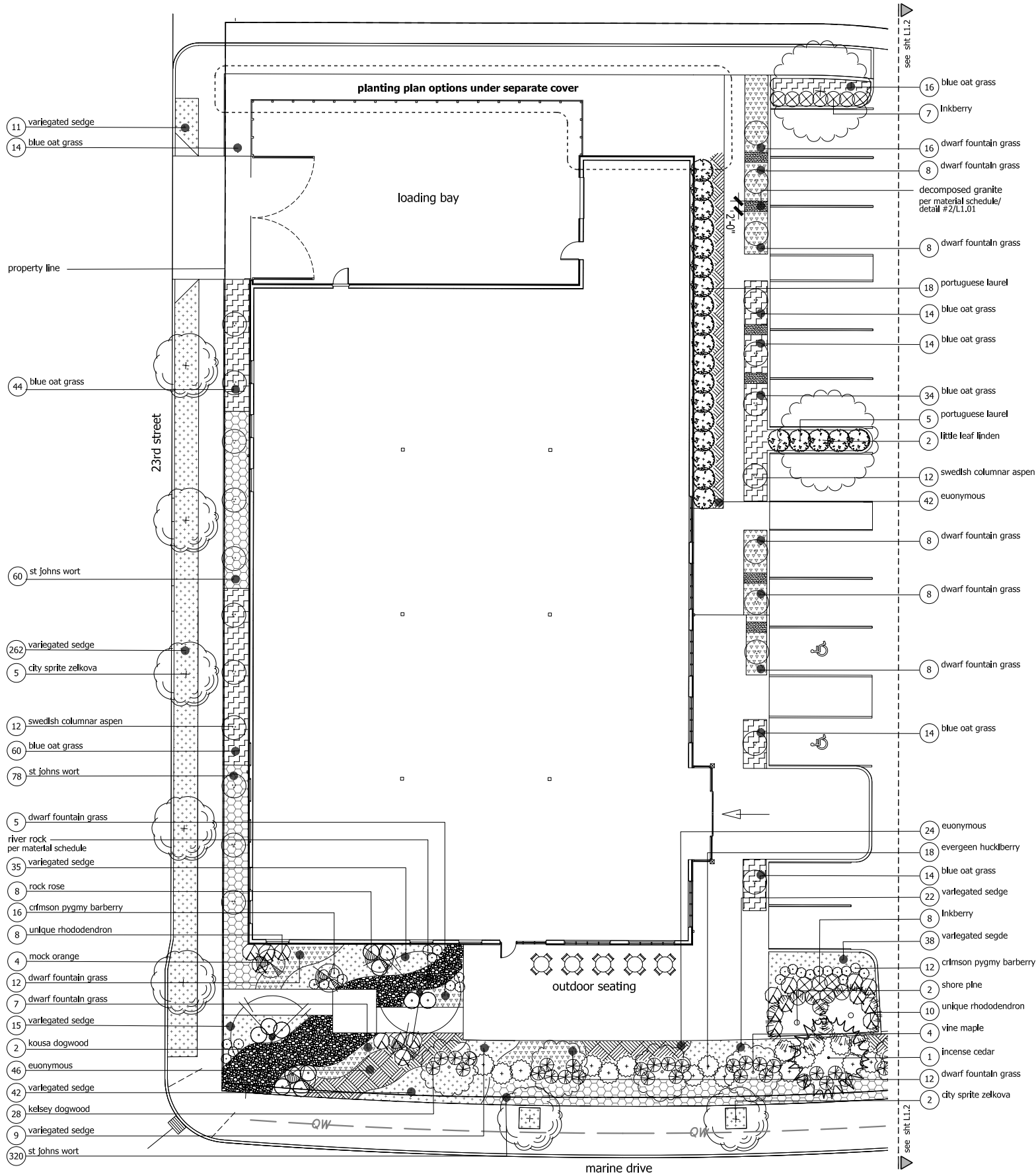


METAL ARC BIKE RACK  
Color: STEEL  
Finish: BRUSHED  
Location: EAST ELEVATION



general notes: landscape plan

- the contractor shall verify with owner and utility companies the locations of all utilities prior to construction. the contractor shall determine in the field the actual locations and elevations of all existing utilities whether shown on the plans or not. the contractor shall call utility protection service 72 hours prior to construction.
- the contractor shall examine finish surface, grades, topsoil quality and depth. do not start any work until unsatisfactory conditions have been corrected. verify limits of work before starting.
- contractor to report all damages to existing conditions and inconsistencies with plans to designated representative.
- all plant masses to be contained within a bark mulch bed, unless noted otherwise.
- bed edge to be no less than 12" and no more than 18" from outer edge of plant material branching, where ground-cover occurs, plant to limits of area as shown.
- contractor shall maintain positive drainage in all landscape beds and all lawn areas.
- contractor to fine grade and rock-hound all turf areas prior to seeding, to provide a smooth and continual surface, free of irregularities (bumps or depressions) & extraneous material or debris.
- quantities shown are intended to assist contractor in evaluating their own take-offs and are not guaranteed as accurate representations of required materials. the contractor shall be responsible for his bid quantities as required by the plans and specifications. if there is a discrepancy between the number labeled on the plant tag and the quantity of graphic symbols shown, the graphic symbol quantity shall govern.
- coordinate landscape installation with installation of underground sprinkler and drainage systems.
- with the exception of those trees indicated on the tree removal plan, contractor shall not remove any trees during construction without the express written consent of the designated representative. existing vegetation to remain shall be protected as directed by the designated representative.
- where proposed tree locations occur under existing overhead utilities or crowd existing trees, notify designated representative to adjust tree locations.
- landscape maintenance period begins immediately after the completion of all planting operations and written notification to the designated representative. maintain trees, shrubs, lawns and other plants until final acceptance or 90 days after notification and acceptance, whichever is longer.
- remove existing weeds from project site prior to the addition of organic amendments and fertilizer. apply amendments and fertilizer per the recommendations of the soil analysis from the site.
- back fill material for tree and shrub planting shall contain: one part fine grade compost to one part topsoil by volume, bone meal per manufacturer's recommendation, and slow release fertilizer per manufacturer's recommendation.
- ground covers and perennials shall be planted with a maximum 2 inch cover of bark mulch with no foliage covered.
- contractor shall obtain written approval for all plant material substitutions from the landscape architect prior to installation. plant substitutions without prior written approval that do not comply with the drawings and specifications may be rejected by the landscape architect at no cost to the owner. these items may be required to be replaced with plant materials that are in compliance with the drawings.
- all plant materials shall be nursery grown with healthy root systems and full branching, disease and insect free and without defects such as sun scald, abrasions, injuries and disfigurement.
- all plant material shall be installed at the size and quantity specified. the landscape architect is not responsible for sub-standard results caused by reduction in size and/or quantity of plant materials.



1017 SW Harrison, Suite 205  
Astoria, OR 97103  
t: 503-841-6315  
c: 503-504-6587

**simpl**  
a land design workshop

REGISTERED  
349

JEFFREY K. SIMPSON  
OREGON  
03/31/19  
LANDSCAPE ARCHITECT

astoria coop  
23rd street & marine drive  
astoria, oregon 97103

project no.  
VCA0217

sheet title  
landscape  
plan

date  
designed 03.26.18  
drawn  
checked






rev. date by

sheet  
L1.1



plant materials schedule

common name	botanical name	size	spacing	remark
<b>trees</b>				
city sprite zelkova	zelkova serrata "jfs-kw1"	2" cal.	30'-0" o.c.	similar cultivar acceptable. all plants must be the same cultivar.
shore pine	pinus contorta var. contorta	6'-8'	as shown	
incense cedar	calocedrus decurrens	6'-8'	as shown	
honey locust	gleditsia triacanthos	2" cal.	26'-0" o.c.	
little leaf linden	tilia cordata	2" cal.	as shown	
columnar swedish aspen	populus tremula 'erecta'	2" cal.	as shown	
kousa dogwood	cornus kousa	2" cal.	as shown	
vine maple	acer circinatum	6' hgt. multistem	as shown	
<b>shrubs</b>				
mock orange	philadelphus lewisii	5 gal.	as shown	
kelsey dogwood	cornus sericea "kelseyi"	5 gal.	2'-0" o.c.	
Inkberry	illex glabra 'shamrock'	3 gal.	as shown	
portuguse laurel	prunus lusitanica	5 gal.	4'-0" o.c.	
evergreen huckleberry	vacinium ovatum	5 gal.	as shown	
unique rhododendron	rhododendron 'unique'	5 gal.	2'-6" o.c.	
rock rose	cistus (various species)	3 gal.	as shown	
crimson pygmy barberry	berberis thunbergii	3 gal.	as shown	


groundcover

	st. john's wort	hypericum caldnum "aaron's beard"	1 gal.	18" o.c. tri.
	blue oat grass	helictotrichon sempervirens	1 gal.	18" o.c. tri.
	dwarf fountain grass	pennisetum alopecuroides 'hameln'	1 gal.	24" o.c. tri.
	euonymus	euonymus fortunei	1 gal.	18" o.c. tri.
	variegated sedge	carex morrowii 'ice dance'	1 gal.	18" o.c. tri.

granular material

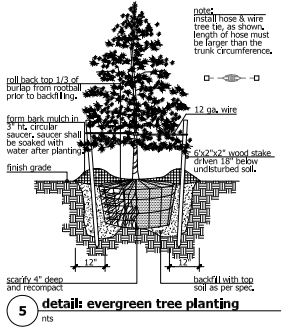
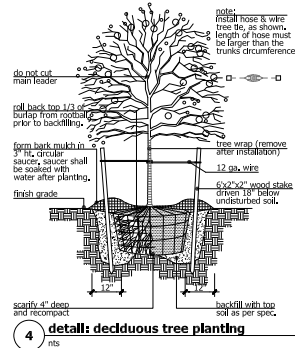
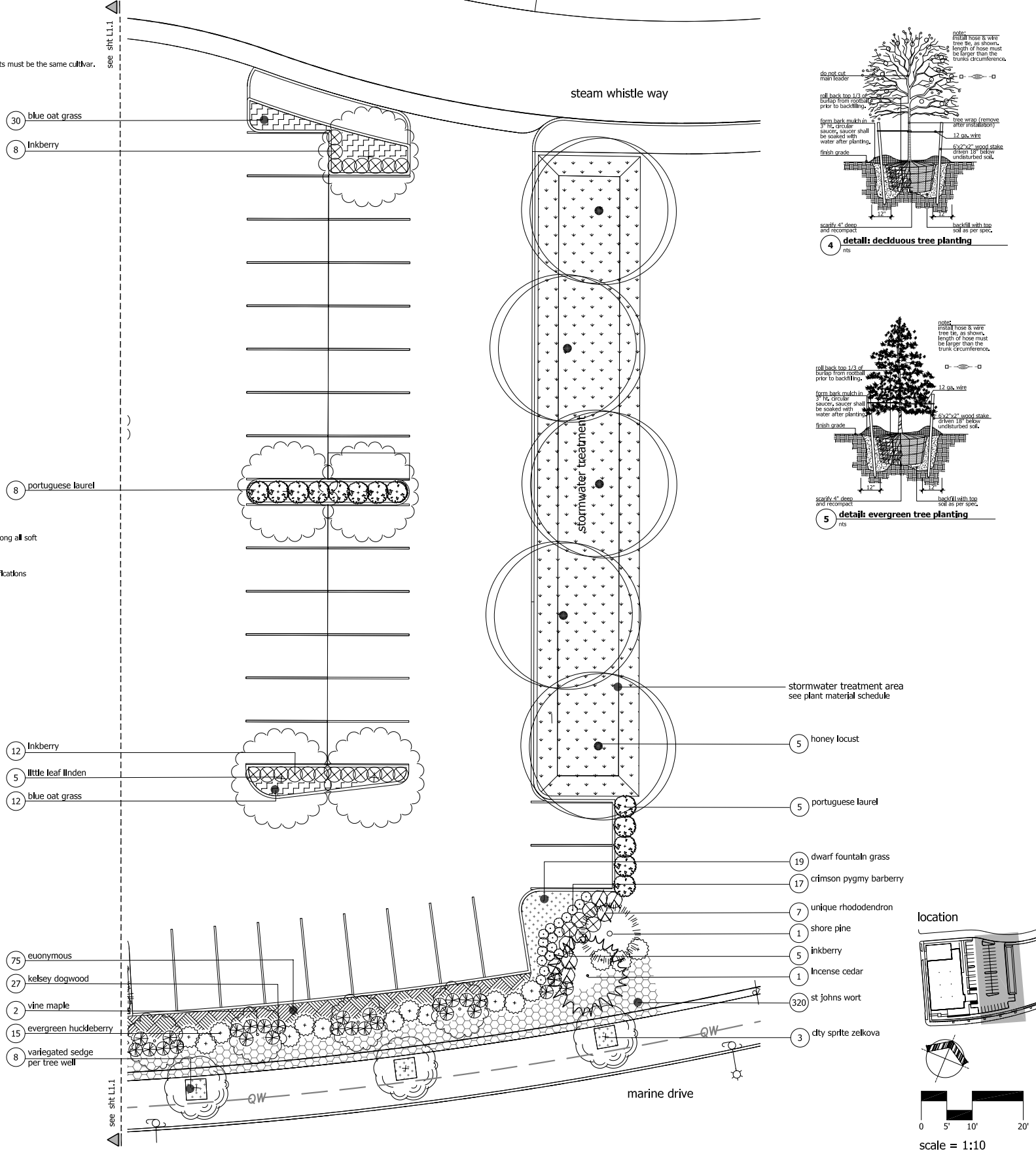
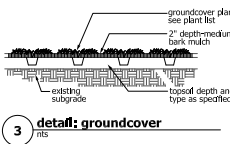
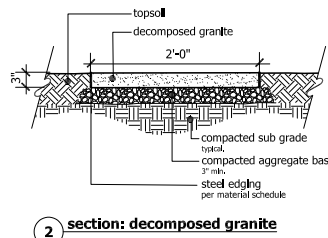
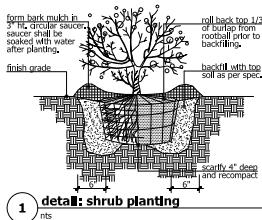
	river rock (310sf approx.)	size: 2'-6" mfg: locally sourced	
	decomposed granite	depth: 3" min. mfg: locally sourced dimension: 2ft x width of planter	

stormwater treatment

	vegetated swale schedule (13,800sf approx.)		qty / per 10sf
mahonia nervosa	dwarf oregon grape	1 gal.	24" o.c. (group*) triangular spacing 1 (1380 approx.)
bay blue rush	juncus effusus 'bay blue'	4" pots	16" o.c. (group*) triangular spacing 2 (2760 approx.)
ovate spiked rush	eleocharis ovata	1-1/2" plugs	16" o.c. (group*) triangular spacing 2 (2760 approx.)
berkeley sedge	carex tumulicola	1-1/2" plugs	16" o.c. (group*) triangular spacing 2 (2760 approx.)
rossi sedge	carex rossi	1-1/2" plugs	16" o.c. (group*) triangular spacing 2 (2760 approx.)
elks blue rush	juncus patens	1-1/2" plugs	16" o.c. (group*) triangular spacing 1 (1380 approx.)

\* "group" can include up to 12 plants. contractors discretion.

note: quantities shown are intended to assist contractor in evaluating their own take-offs and are not guaranteed as accurate representations of required materials. the contractor shall be responsible for his bid quantities as required by the plans and specifications. if there is a discrepancy between the number labeled on the plant tag and the quantity of graphic symbols shown, the graphic symbol quantity shall govern



astoria coop  
23rd street & marine drive  
astoria, oregon 97103

project no. VCA0217  
sheet title landscape plan  
date designed 03.26.18  
date drawn  
date checked  
rev. 0000  
sheet L1.2








plant materials schedule



common name	botanical name	size	spacing	remark
<b>trees</b>				
city sprite zelkova	zelkova serrata "jfs-kw1"	2" cal.	30'-0" o.c.	similar cultivar acceptable. all plants must be the same cultivar.
shore pine	pinus contorta var. contorta	6'-8"	as shown	
incense cedar	calocedrus decurrens	6'-8"	as shown	
honey locust	gleditsia triacanthos	2" cal.	26'-0" o.c.	
little leaf linden	tillia cordata'	2" cal.	as shown	
columnar swedish aspen	populus tremula 'erecta'	2" cal.	as shown	
kousa dogwood	cornus kousa	2" cal.	as shown	
vine maple	acer circinatum	6' hgt. multistem	as shown	

<b>shrubs</b>				
mock orange	philadelphus lewisii	5 gal.	as shown	
kelsey dogwood	cornus serciea "kelseyi"	5 gal.	2'-0" o.c.	
inkberry	ilex glabra 'shamrock'	3 gal.	as shown	
portuguse laurel	prunus lusitanica	5 gal.	4'-0" o.c.	
evergreen huckleberry	vaccinium ovatum	5 gal.	as shown	
unique rhododendron	rhododendron 'unique'	5 gal.	2'-6" o.c.	
rock rose	cistus (various species)	3 gal.	as shown	
crimson pygmy barberry	berberis thunbergii	3 gal.	as shown	

groundcover

	st. john's wort	hypericum calcinum "aaron's beard"	1 gal.	18" o.c. tri.
	blue oat grass	helictotrichon sempervirens	1 gal.	18" o.c. tri.
	dwarf fountain grass	pennisetum alopecuroides 'hameln'	1 gal.	24" o.c. tri.
	euonymus	euonymous fortunei	1 gal.	18" o.c. tri.
	variegated sedge	carex morrowii 'ice dance'	1 gal.	18" o.c. tri.


granular material

	river rock (310sf approx.)	size: 2'-6" mfg: locally sourced		
	decomposed granite	depth: 3" min. mfg: locally sourced dimension: 2ft x width of planter		

remark

requires a steel edging restraint along all soft borders;  
mfg: j.d.russell co.  
color: black  
model: dura edge  
notes: install per mfg, specifications

stormwater treatment

	vegetated swale schedule (13,800sf approx.)				qty / per 10sf
	mahonia nervosa	dwarf oregon grape	1 gal.	24" o.c. (group*) triangular spacing	1 (1380 approx.)
	bay blue rush	juncus effusus 'bay blue'	4" pots	16" o.c. (group*) triangular spacing	2 (2760 approx.)
	ovate spiked rush	eleocharis ovata	1-1/2" plugs	16" o.c. (group*) triangular spacing	2 (2760 approx.)
	berkley sedge	carex tumulicola	1-1/2" plugs	16" o.c. (group*) triangular spacing	2 (2760 approx.)
	rossi sedge	carex rossi	1-1/2" plugs	16" o.c. (group*) triangular spacing	2 (2760 approx.)
	elks blue rush	juncus patens	1-1/2" plugs	16" o.c. (group*) triangular spacing	1 (1380 approx.)

\* "group" can include up to 12 plants. contractors discretion.  
note: quantities shown are intended to assist contractor in evaluating their own take-offs and are not guaranteed as accurate representations of required materials. the contractor shall be responsible for his bld qnttities as required by the plans and specifications. if there is a discrepancy between the number labeled on the plant tag and the quantity of graphic symbols shown, the graphic symbol quantity shall govern



UNIQUE  
RHODODENDRON



DOUBLEFILE  
VIBURNUM



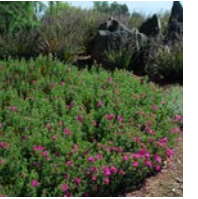
INKBERRY



ST JOHNS WORT



DWARF MOUNTAIN  
LAUREL



ROCK ROSE



VARIEGATED  
SEDE



DWARF FOUNTAIN  
GRASS



MOCK ORANGE



AUTUMN MOOR  
GRASS



EVERGREEN  
HUCKLEBERRY



RED TWIG  
DOGWOOD



KELSEY  
DOGWOOD



DWARF KOREAN  
LILAC



BLUE OAT GRASS



SHORE PINE



KOUSA DOGWOOD



CITY SPRITE  
ZELKOVA



SWEDISH  
COLUMNAR ASPEN



LITTLE LEAF  
LINDEN



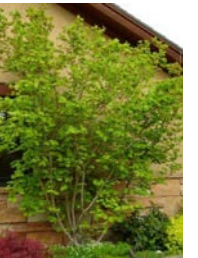
INCENSE CEDAR



WESTERN HEMLOCK



HONEY LOCUST



VINE MAPLE



STORM WATER TREATMENT AREA



**schematic "A"**

vinyard trellis (3 sections @ 16' ea)

- wood posts
- cable w/ turnbuckles
- anchor guys @ each end

16 unique rhododendron

33 pachysandra

3 portuguese laurel

11 variegated sedge

9 blue oat grass

steam whistle way

loading bay

33 pachysandra

5 swedish columnar aspen

33 pachysandra

1 vine maple

18 kelsey dogwood

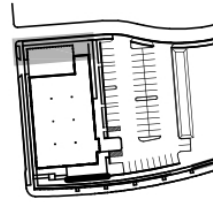
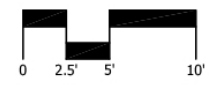
1 shore pine

1 kousa dogwood

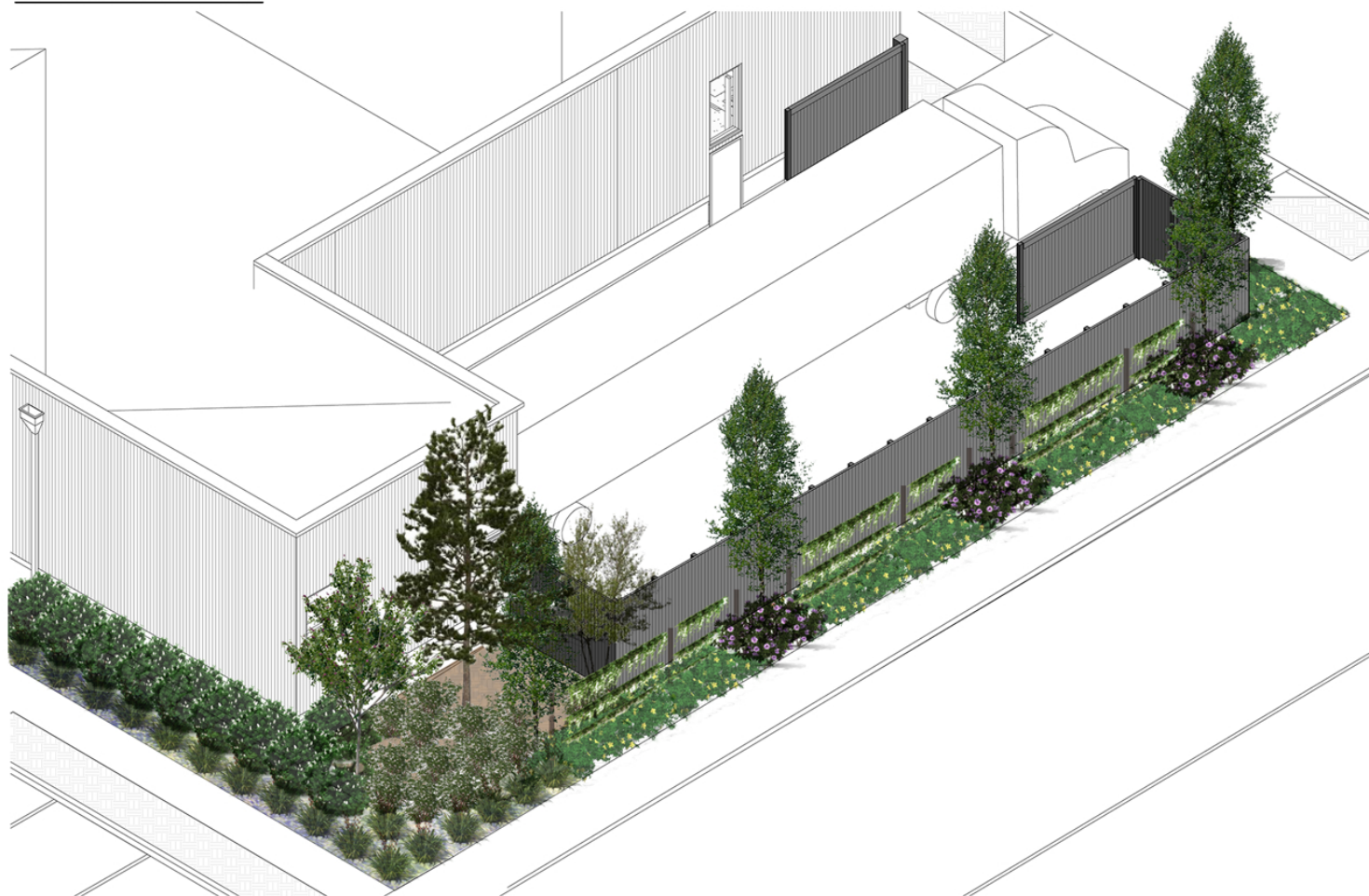
26 euonymous

4 portuguese laurel

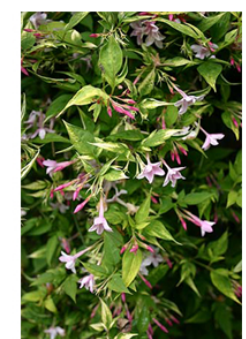
plan view:  
scale 1:10



**axonometric view**



**precedent images**



jasmine



wisteria



portuguese laurel



vineyard trellis

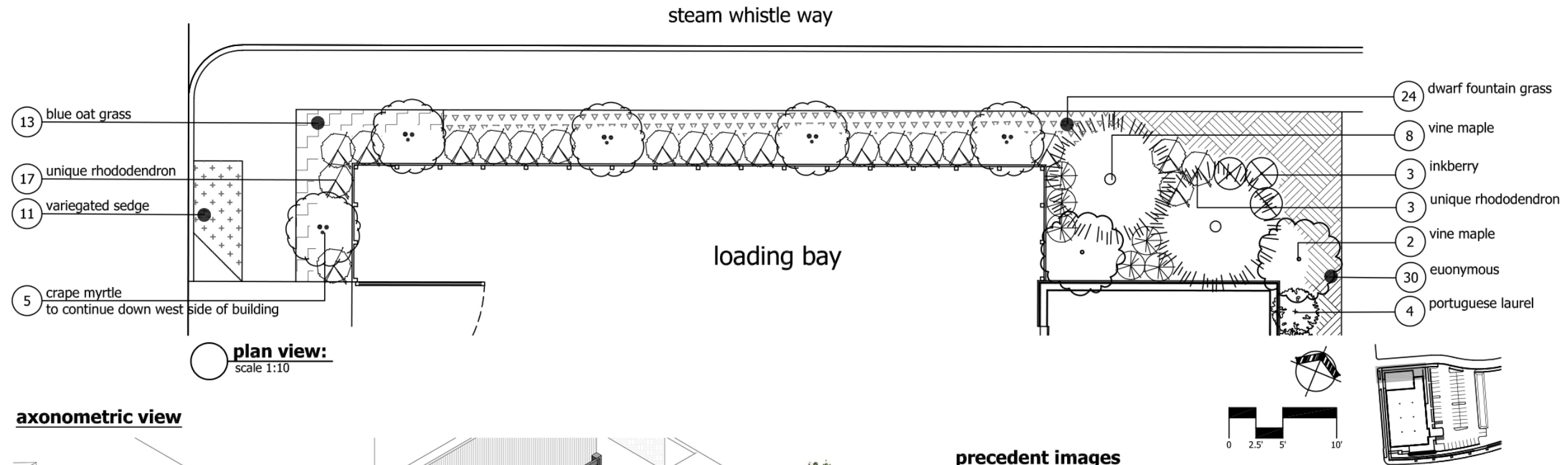


swedish columnar aspen

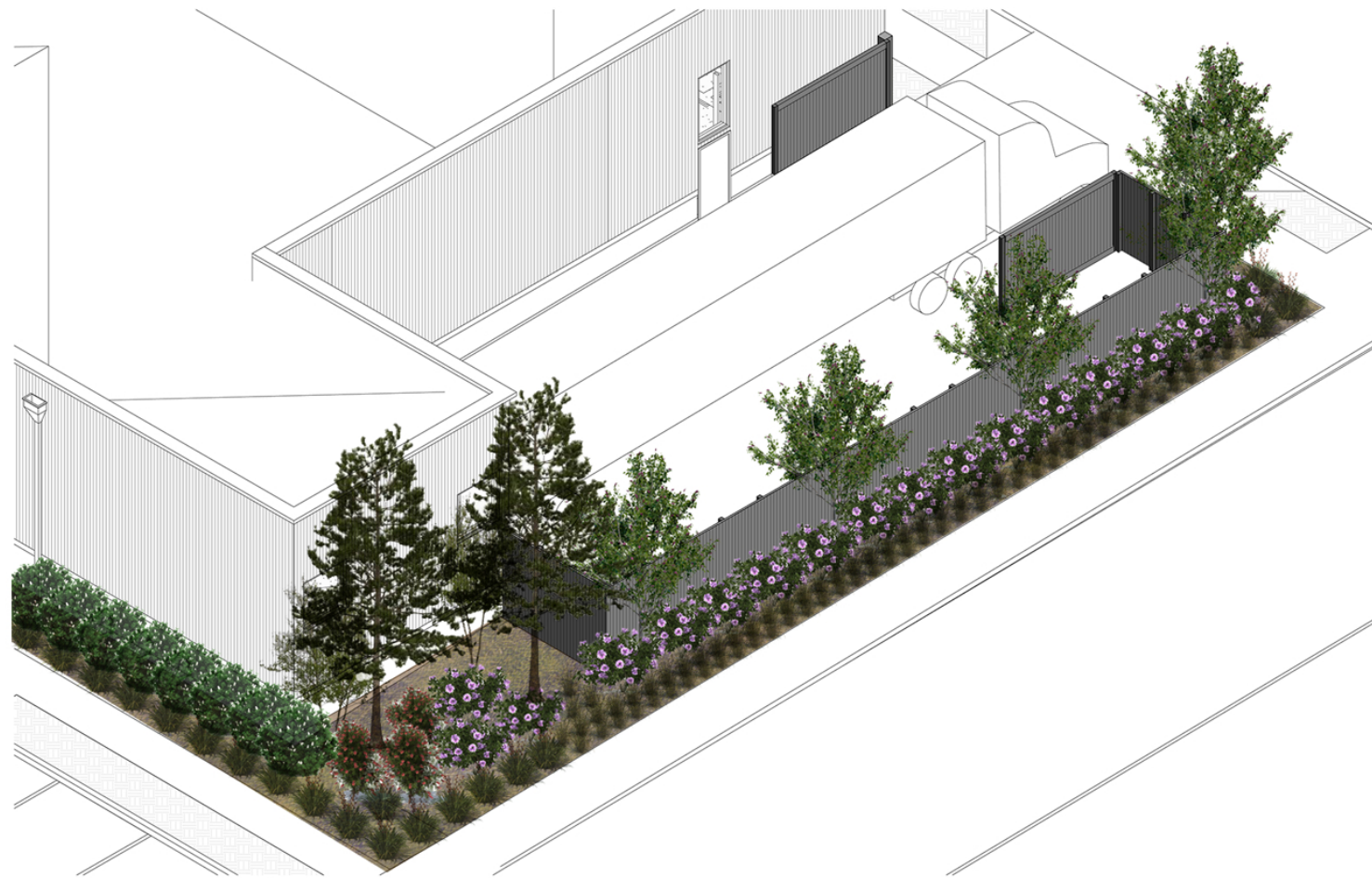
a land design workshop



**schematic "B"**



**axonometric view**



**precedent images**



crape myrtle



dwarf fountain grass



unique rhododendron



shore pine

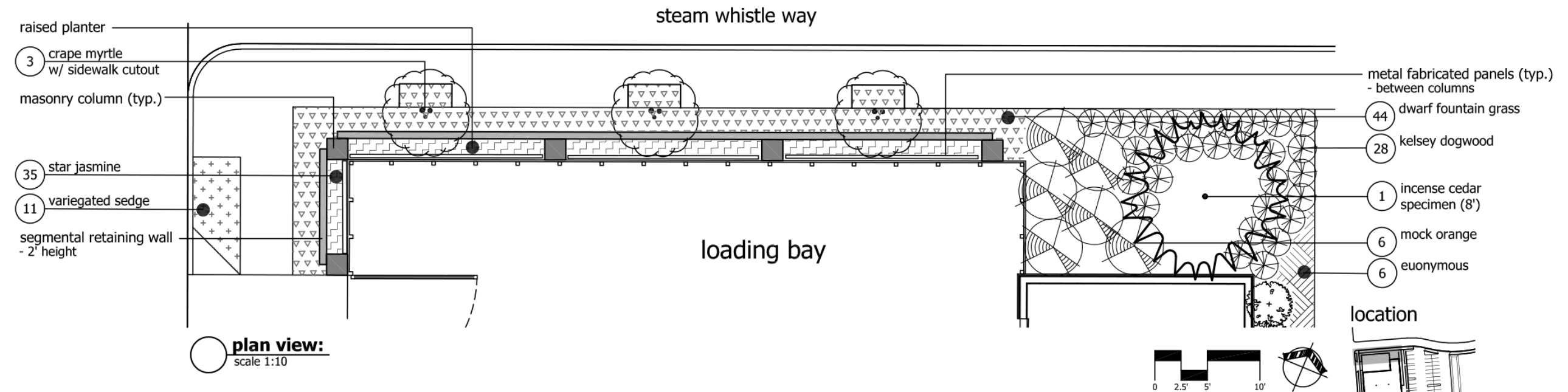


vine maple

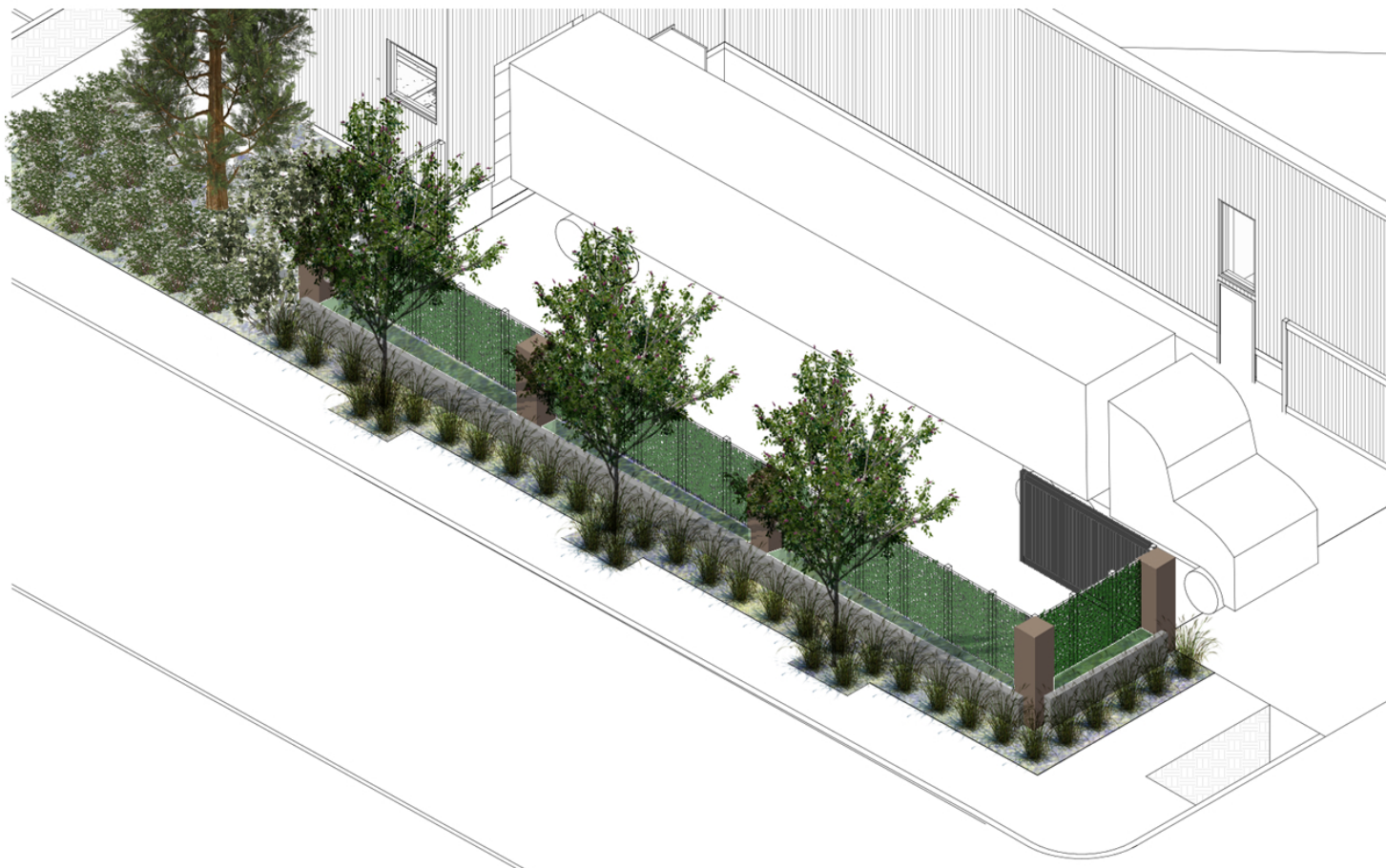
a land design workshop



**schematic "C"**



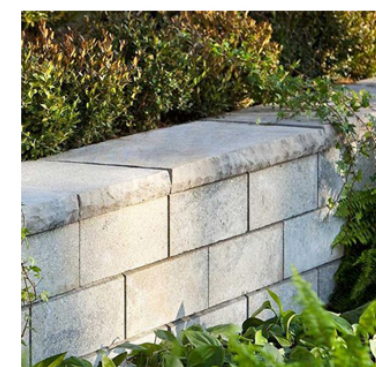
**axonometric view**



**precedent images**



jasmine



segmental retaining wall



incense cedar



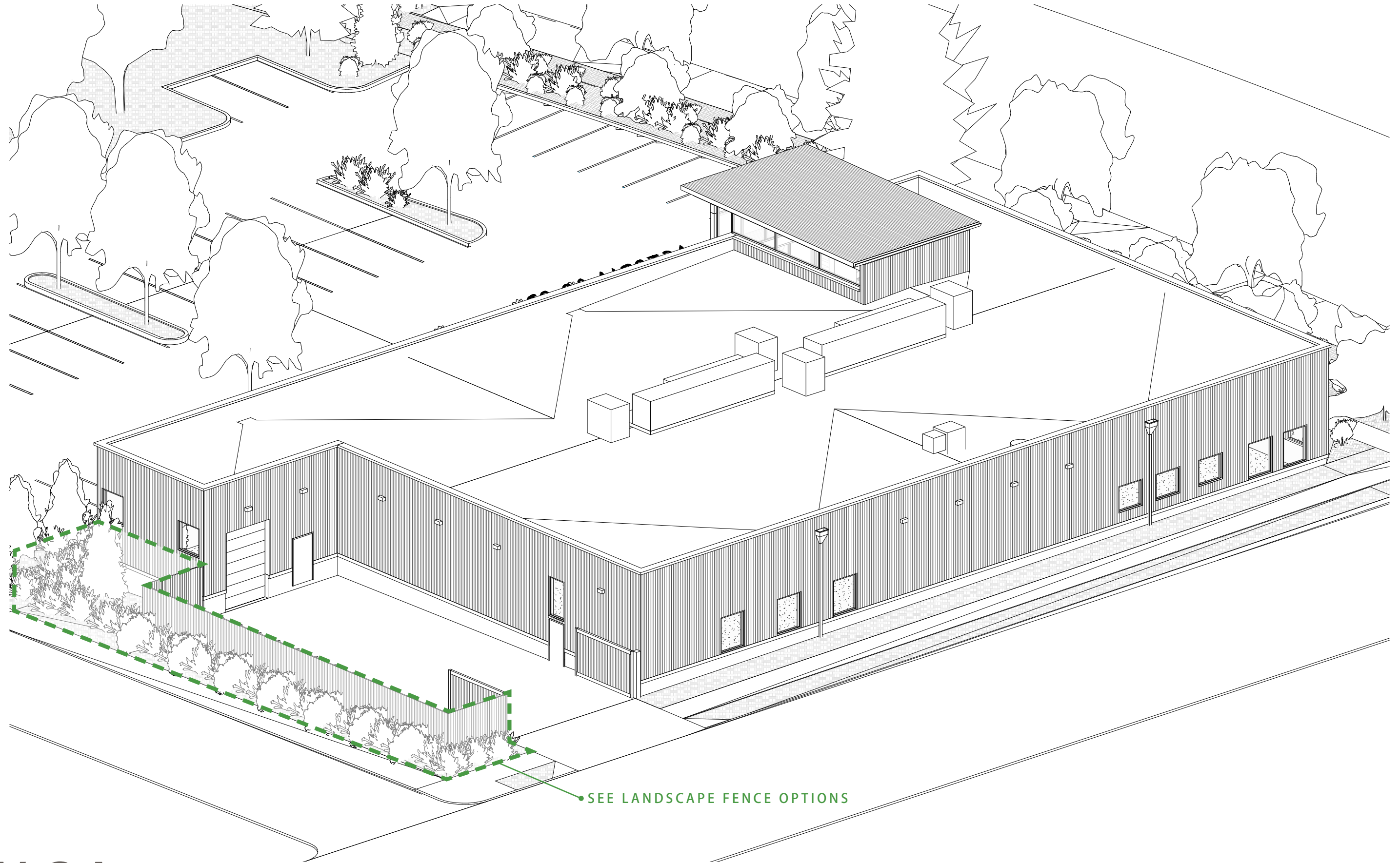
metal fabric panel



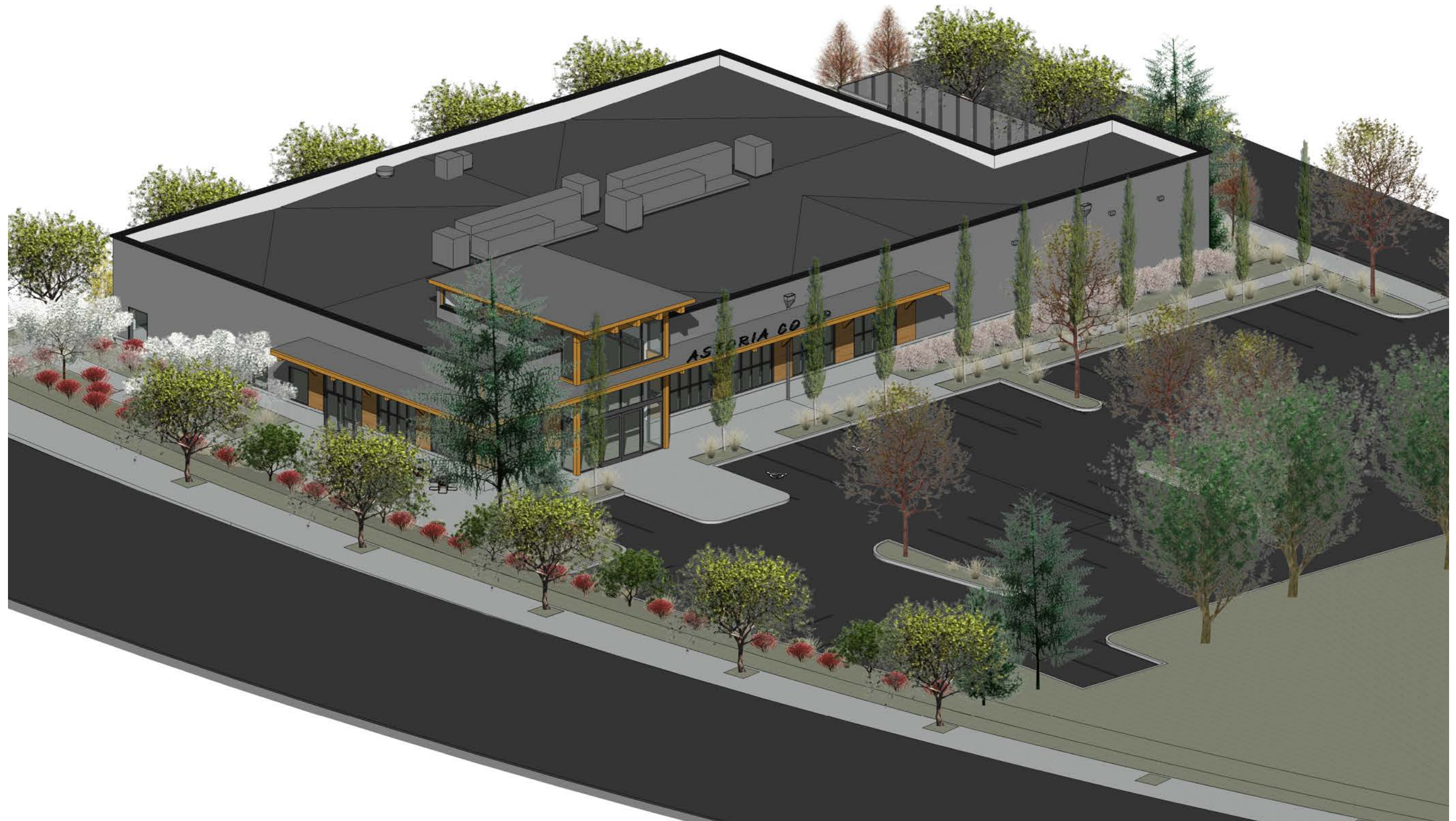
kelsey dogwood

a land design workshop













SE PERSPECTIVE



NW PERSPECTIVE





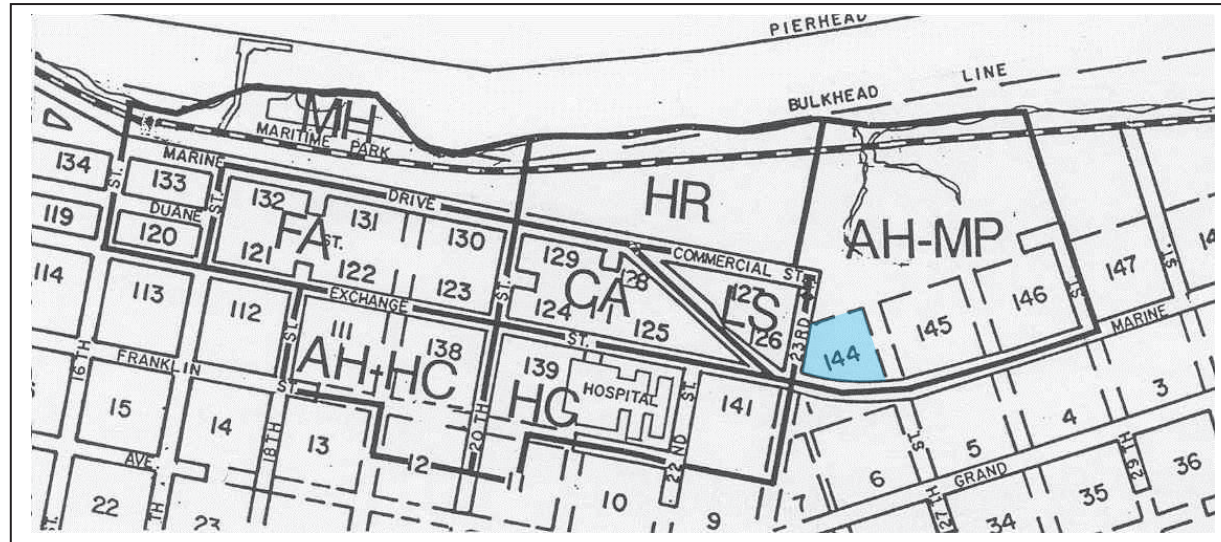
S PERSPECTIVE



NE PERSPECTIVE



City of Astoria  
Development Code



Gateway Overlay Zones (see Land Use and Zoning Map for actual zone boundaries)

B. Design Review.

Each public or private development proposal within the Gateway Overlay Zone will be reviewed for consistency with the Design Review Guidelines in Sections 14.020 through 14.030.

14.020. APPLICABILITY OF DESIGN REVIEW GUIDELINES.

The Design Review Guidelines shall apply to all new construction or major renovation. For the purposes of this Code Section, “major renovation” is defined as construction valued at 25% or more of the assessed value of the existing structure. The guidelines are intended to provide fundamental principles that will assist in the review of the proposed development. The principles identify both “encouraged” and “discouraged” architectural elements. They are broad design objectives and are not to be construed as prescriptive standards.

14.025. DESIGN REVIEW GUIDELINES.

A. Purpose.

These guidelines promote architectural elements that unify the Gateway Area by encouraging styles characteristic of Astoria. The historic architecture of Astoria is represented by a variety of styles. Differences in details may be seen from one neighborhood to the next. These guidelines advocate the simplicity of design which is characteristic of Uppertown and the working waterfront. Building styles and details not inspired by Astoria’s past will be discouraged. Monotony of design should be avoided. Variety of detail, form, and siting should be used to provide visual interest.

The Gateway Plan encourages new construction to reflect historic building types found in the Uppertown area. Three historic building types commonly found in the area include waterfront industrial, commercial, and residential. These building types may be used as models for contemporary building design, but do not restrict or define their function.

The Guidelines make reference to, but do not require the use of, historic materials. Contemporary substitutions (i.e. composite materials), will not be discouraged if their texture, profile, and proportions are similar to those materials with historic precedent.

Figures included in Section 14.015 through 14.030 are included for illustrative purposes only and are not intended to be regulatory in and of themselves. If there is an inconsistency between the Figure and the Development Code text, the text shall prevail.

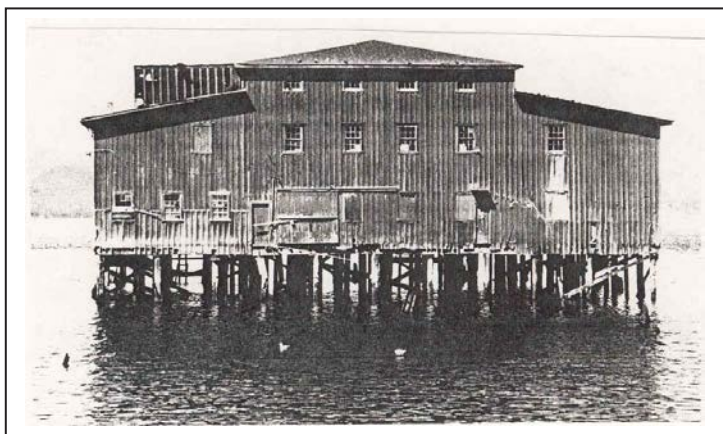
B. Building Forms Encouraged.

1. All Building Types.

- a. Simple designs without extraneous details.
- b. Rectangular in plan.
- c. Square in plan.

2. Waterfront Industrial.

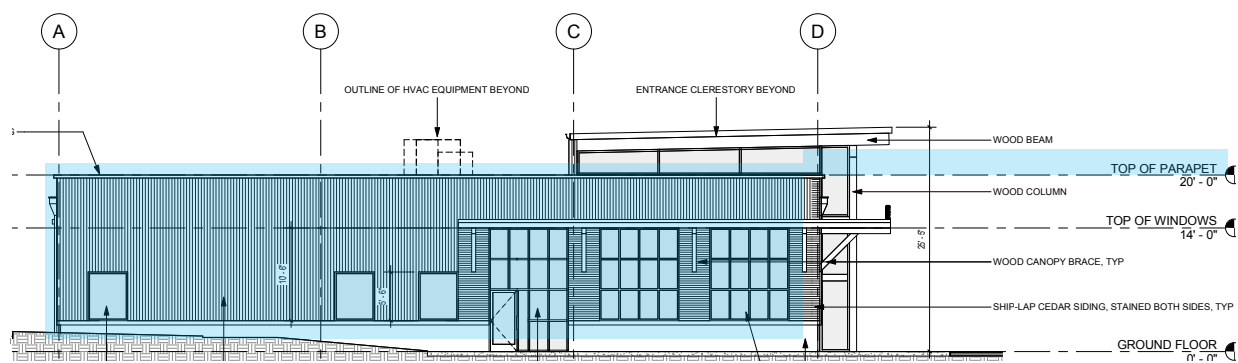
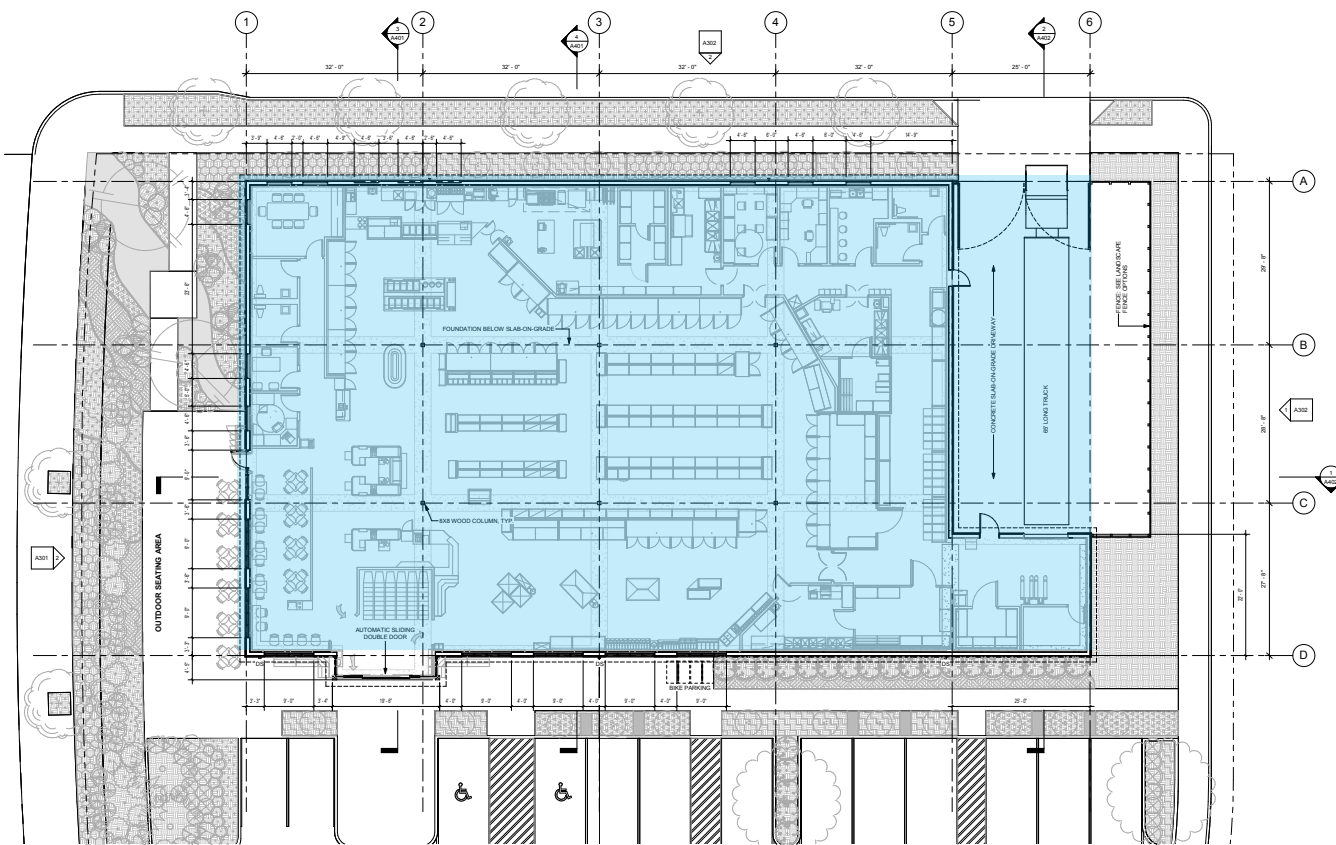
- a. Low in form.
- b. Cubic in form.



3. Commercial.

- a. Low in form.

Figure B.3.





## D. Windows Encouraged.

### 1. All Building Types.

- a. True-divided, multiple-light windows.
- b. Single-light windows.
- c. Applied muntins with profile facing window exterior.
- d. Rectangular windows with vertical proportions.
- e. Fixed windows.
- f. Double or single-hung windows.
- g. Casement windows.
- h. Windows should be spaced and sized so that wall area is not exceeded by window area, with the exception of commercial storefronts.

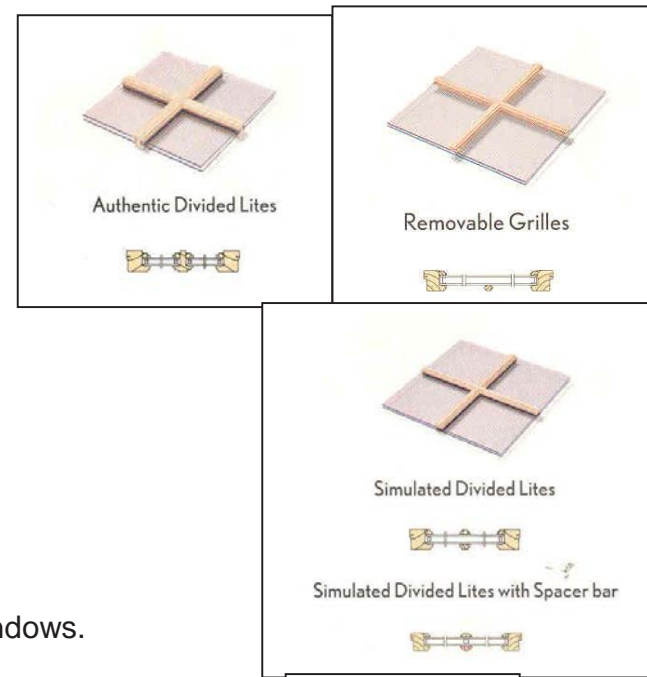


Figure D.1.c.

### 3. Commercial.

#### a. Storefronts.

- 1) Plate glass windows with multiple-light transom windows above.
- 2) Recessed entries.
- 3) Window to wall surface proportions may be exceeded.



Figure D.3.

## E. Windows Discouraged.

### 1. All Building Types.

- a. Applied muntins which have no profile.
- b. Smoked glass.
- c. Mirrored glass.
- d. Horizontal sliding windows.
- e. Walls predominated by large expanses of glass, except in commercial storefronts.
- f. Windowless walls. Large expanses of blank walls should only be located in areas which are not visible to the public.
- g. Aluminum frame windows, except in commercial storefronts.

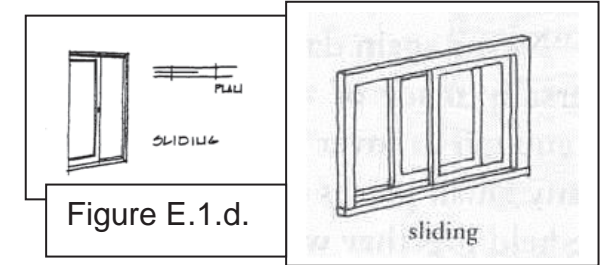
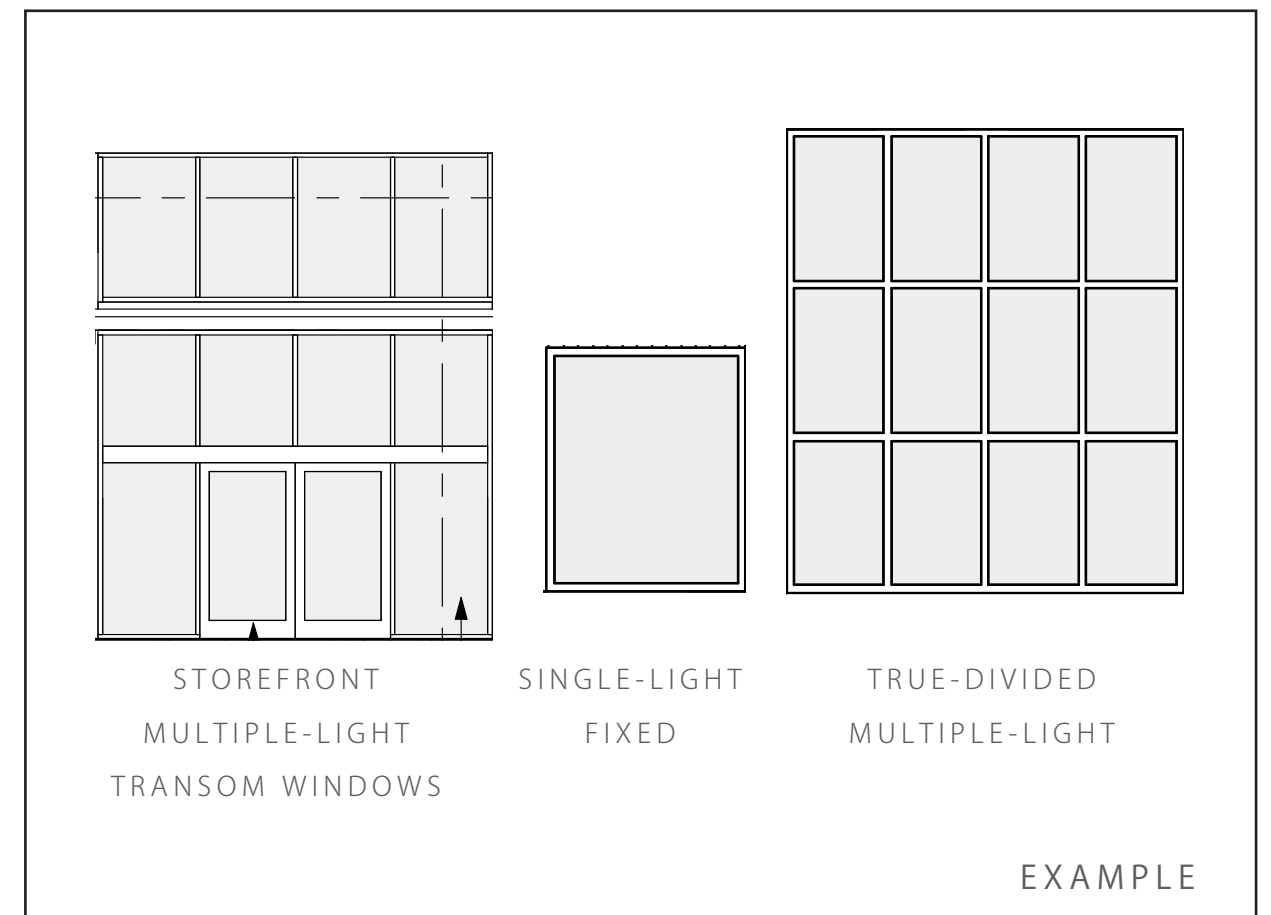


Figure E.1.d.





## F. Exterior Wall Treatments Encouraged.

### 1. All Building Types.

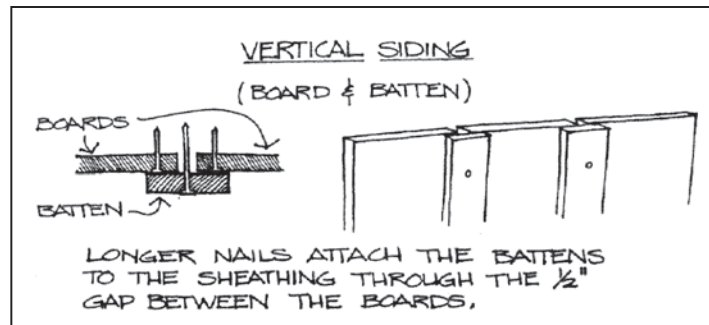
- Drop siding.
- Weatherboard siding.
- Horizontal siding with six inches or less exposure.

### 2. Waterfront Industrial.

- Board and batten style.
- Galvanized corrugated metal.



Figure F.2.



(Exterior Wall Treatments Encouraged)

### 3. Commercial.

- Finished concrete.
- Brick veneer.

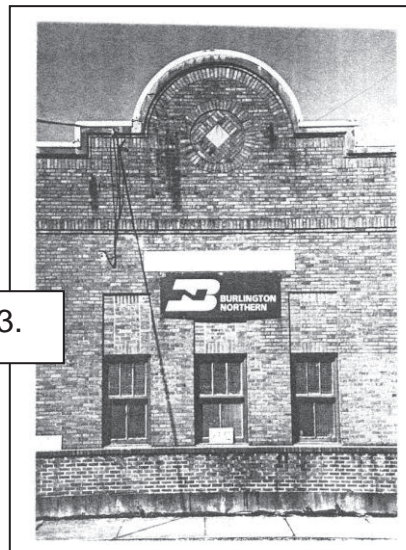


Figure F.3.

## G. Exterior Wall Treatments Discouraged.

### 1. All Building Types.

- Exposed textured, concrete block.
- Flagstone or other applied stone products.
- Precast concrete or decorative concrete panels.
- Wood shakes.
- Plywood paneling.

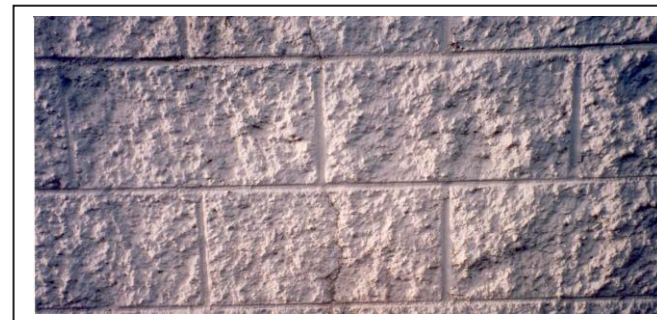
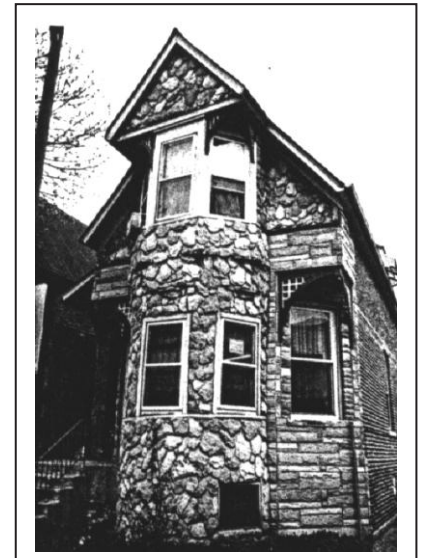


Figure G.1.a.

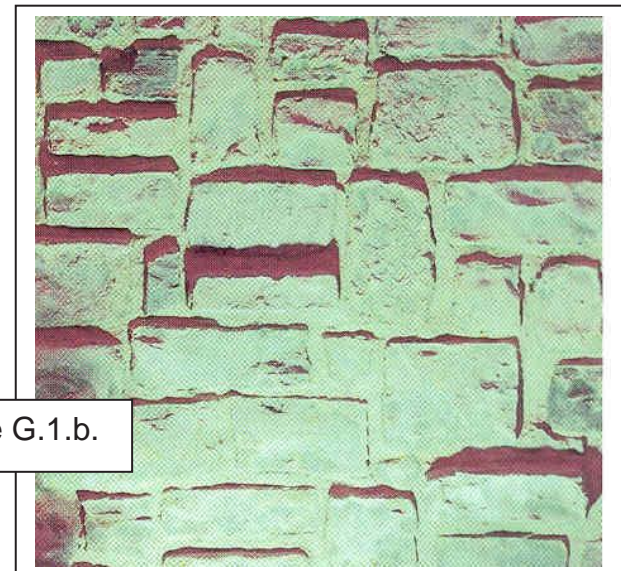


Figure G.1.b.

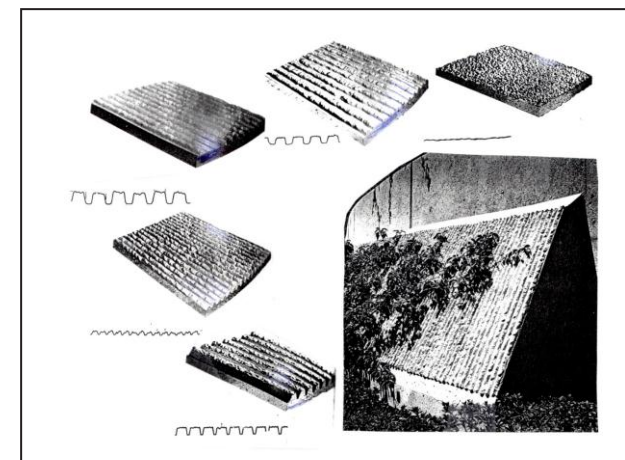
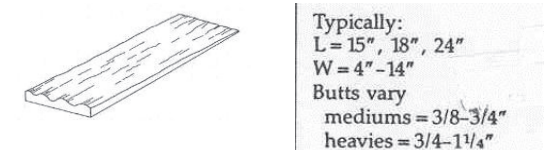


Figure G.1.c.



Commercially available. Machine split and sawn on the backs to taper. Split faces often irregular, even corrugated in appearance. Butt thickness vary and may be too wide.

Figure G.1.d.



## H. Roof Elements Encouraged.

1. Waterfront Industrial.
  - a. Single gable with low pitch.
  - b. Repetitive gable with steep pitch.
  - c. Shallow eaves.
  - d. Small shed roof dormers.
  - e. Monitor roof on ridge line.
  - f. Flat panel skylights or roof window.

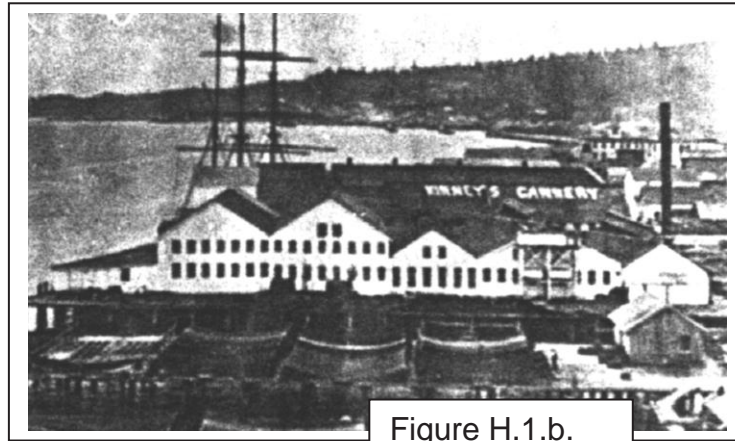


Figure H.1.b.

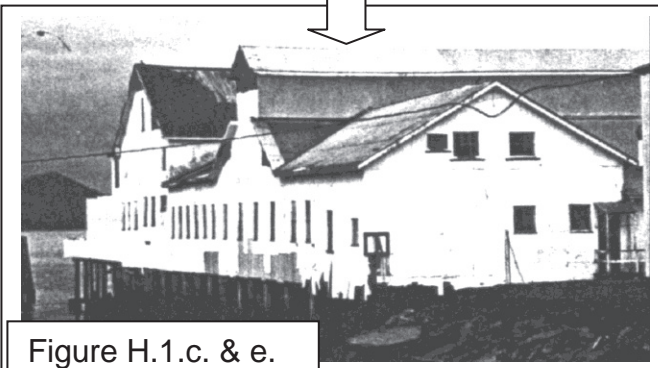


Figure H.1.c. & e.

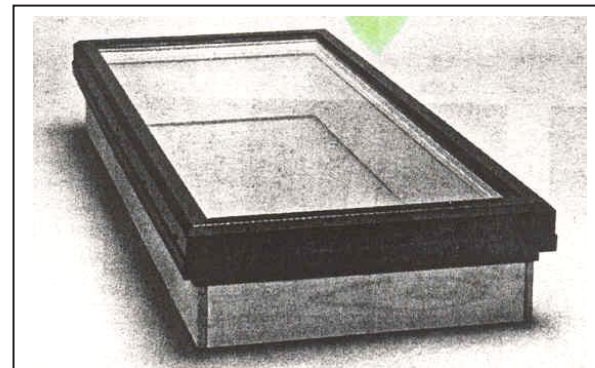


Figure H.1.f.

## 2. Commercial.

- a. Single gable with low pitch.
- b. Repetitive gable with steep pitch.
- c. Shallow eaves behind parapet wall.
- d. Flat or gable roof behind parapet wall.
- e. Structural skylights.

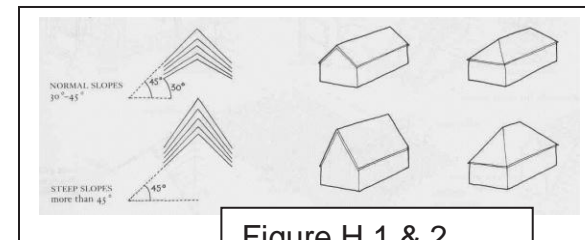


Figure H.1 & 2

## J. Roofing Materials Encouraged.

1. All Building Types.
  - a. Cedar shingle.
  - b. Composition roofing.
  - c. Roofing material in gray, brown, black, deep red, or other subdued colors.

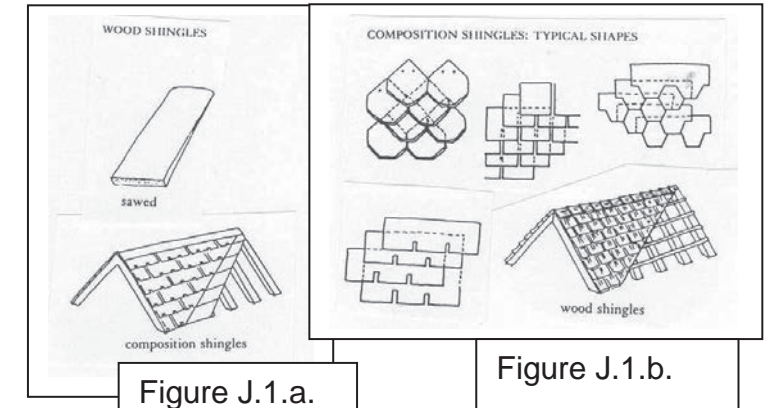


Figure J.1.a.

Figure J.1.b.

## 2. Waterfront Industrial.

- a. Galvanized corrugated metal.
- b. Low profile standing seam, metal roof.
- c. Roll down.

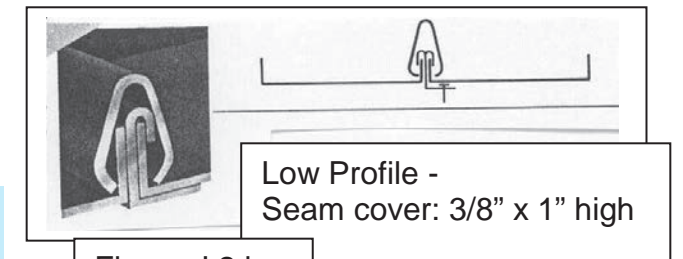


Figure J.2.b.

## 3. Commercial.

- a. Built-up.



## K. Roofing Materials Discouraged.

1. All Building Types.
  - a. High profile standing seam, metal roof.
  - b. Brightly colored roofing material.

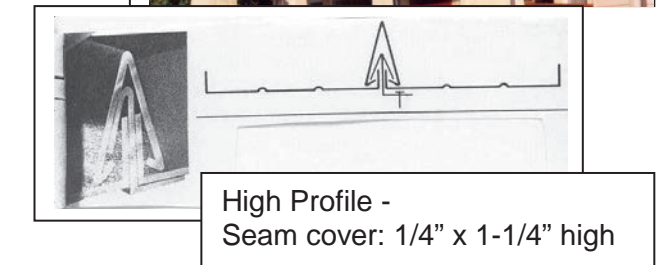
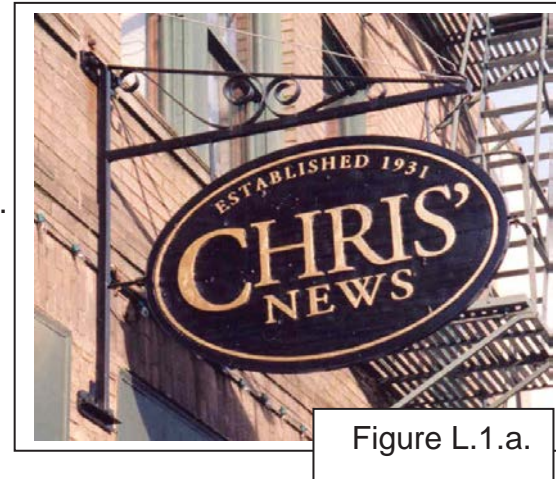


Figure K.1.a.

L. Signs Encouraged.

1. All Building Types.

- a. Hanging blade signs.
- b. Signs painted on building facade.
- c. Signs applied to building facade.
- d. Front lit.
- e. Graphics historic in character.



2. Commercial.

- a. Exterior neon.



M. Signs Discouraged.

1. All Building Types.

- a. Pole mounted freestanding signs.
- b. Plastic or internal and back lit plastic.



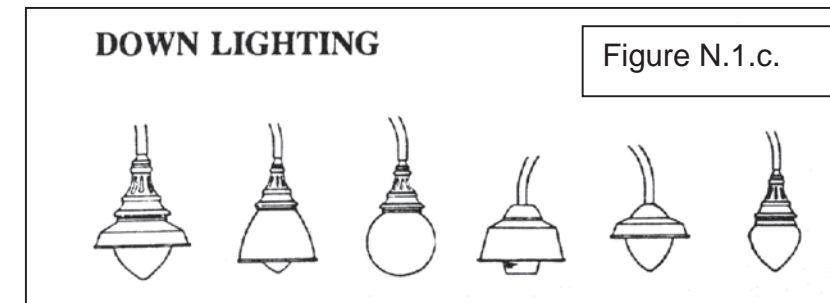
N. Exterior Lighting Encouraged.

1. All Building Types.

- a. Decorative lighting integrated with architecture.
- b. Metal halide or incandescent.

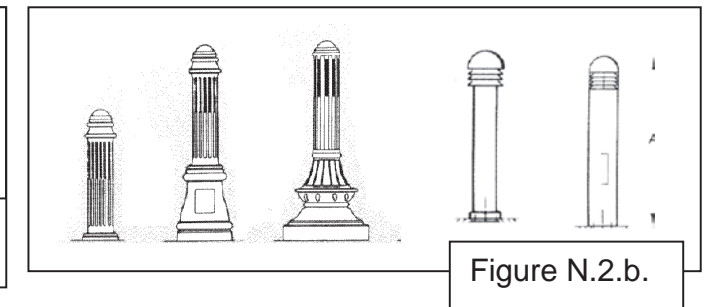
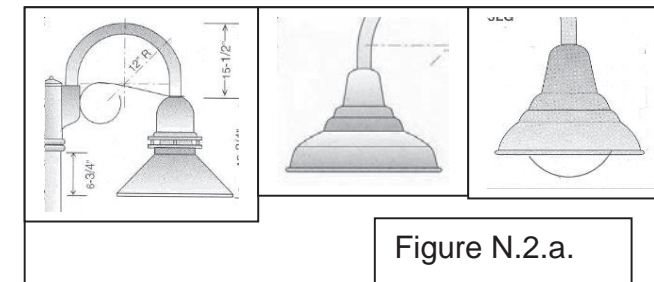
(Exterior Lighting Encouraged)

- c. Pedestrian and traffic signals combined with street lamps.
- d. Light fixtures that direct light downward and eliminate glare.



2. Waterfront Industrial.

- a. Industrial pan light with goose neck.
- b. Low bollard lighting.



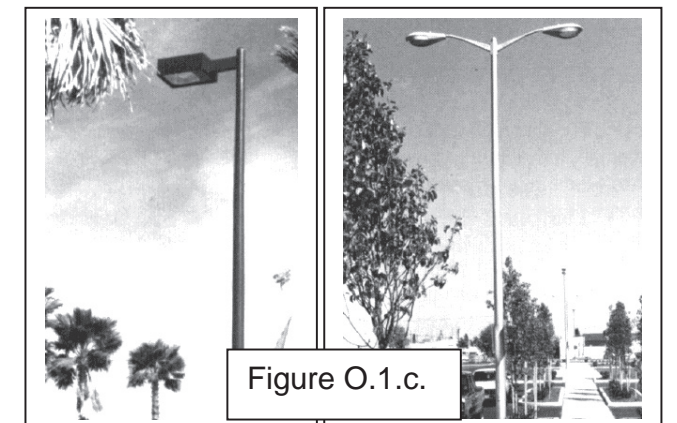
3. Commercial.

- a. Historic street lamps along walks and parking lots.

O. Exterior Lighting Discouraged.

1. All Building Types.

- a. Sodium vapor (amber).
- b. Fluorescent tube.
- c. Cobra head street lamps or other contemporary fixtures.







SOLID STATE AREA LIGHTING

DSDR SERIES-LED

SPECIFICATIONS

**LUMINAIRE**  
Upper housing is 0.125" thick formed alumunim (DSDR1) or heavy wall cast aluminum (DSDR25). Lower housing is 0.080" thick formed aluminum. Closed bottom polycarbonate prismatic refracting lens. Pendant Mount uses Trulevel ball coupling mount. Wall mount has integrated side mount extruded aluminum arm and cast aluminum wall plate. All hardware is stainless steel.

**LED POWER ARRAY™**  
Three-dimensional array of individual LED Tubes fastened to a retaining plate. Each LED Tube consists of circuit board populated with a multiple of LED's and is mechanically fastened to a radial aluminum heat sink. An acrylic lens and end cap protects the LED Tube's internal components.

**VERTICAL POWER ARRAY™ WITH REFRACTING LENS:**  
LED Tubes are aligned vertically and arranged radially to produce an even raw light distribution that simulates standard light sources. Used in conjunction with the external prismatic refracting which provides the optical control.

**LED EMITTERS**  
High Output LED's are driven at 350mA for nominal 1 Watt output each. LED's are available in standard Neutral White (4000K), or optional Cool White (5000K) or Warm White (3000K). Consult Factory for other LED options.

**LED DRIVER**  
Drivers are UL and cUL recognized mounted on a single plate and factory prewired with quick-disconnect plugs. Constant current driver is electronic and has a power factor of >0.90 and a minimum operating temperature of -40°F. Drivers accept an input of 120-277V, 50/60Hz.(0 - 10V dimmable driver is standard. Driver has a minimum of 3KV internal surge protection. Luminaire supplied with 20KV surge protector for field accessible installation.)

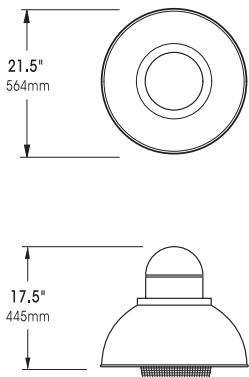
**FINISH**  
Polyester powder coat incorporates four step iron phosphate process to pretreat metal surface for maximum adhesion. Top coat is baked at 400°F for maximum hardness and exterior durability.

PROJECT NAME: \_\_\_\_\_

FIXTURE TYPE: \_\_\_\_\_



DSDR1

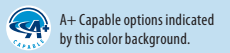


MR1 LED  
LED Area Luminaire



Specifications

- EPA:** 0.54 ft²  
(0.05 m²)
- Length:** 24-1/2"  
(62.2 cm)
- Width:** 18-5/8"  
(47.3 cm)
- Height:** 6-3/8"  
(16.2 cm)
- Weight (max):** 31 lbs  
(14.7 kg)



Ordering Information

EXAMPLE: MR1 LED 42C 700 40K SR5 MVOLT SPA DDBXD

MR1 LED														
Series	LEDs	Drive Current		Color temperature		Distribution				Voltage			Mounting	
MR1 LED	42C 42 LEDs (one engine)	350	350mA	30K	3000K	SR2	Type II	SR4	Type IV	MVOLT <sup>1</sup>	240 <sup>1</sup>	480	SPA	Square pole mounting
		530	530mA	40K	4000K	SR3	Type III	SR5	Type V	120 <sup>1</sup>	277 <sup>1</sup>		RPA	Round pole mounting
		700	700mA	50K	5000K					208 <sup>1</sup>	347		WBA	Wall bracket
Control Options										Other Options			Finish <i>(required)</i>	
Shipped installed						Shipped separately <sup>6</sup>				SF Single fuse (120, 277, 347V) <sup>1</sup>			DDBXD Dark bronze	
PER	NEMA twist-lock receptacle only (no controls)		PNMTDD3	Part night, dim till dawn <sup>5</sup>		VG	Vandal guard		DF Double fuse (208, 240, 480V) <sup>1</sup>			DBLXD Black		
PER5	Five-wire receptacle only (no controls) <sup>2</sup>		PNMT5D3	Part night, dim 5 hrs <sup>5</sup>					DFL Diffusing lens			DNAXD Natural aluminum		
PER7	Seven-wire receptacle only (no controls) <sup>2</sup>		PNMT6D3	Part night, dim 6 hrs <sup>5</sup>								DWHXD White		
DMG	0-10V dimming driver (no controls) <sup>3</sup>		PNMT7D3	Part night, dim 7 hrs <sup>5</sup>								DDBTXD Textured dark bronze		
BL30	Bi-level switched dimming, 30% <sup>4,5</sup>											DBLBXD Textured black		
BL50	Bi-level switched dimming, 50% <sup>4,5</sup>											DNATXD Textured natural aluminum		
												DWHGXD Textured white		

OPTION 1

OPTION 2



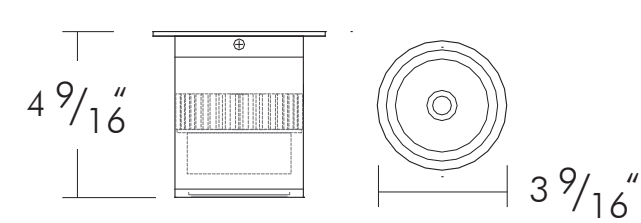
TYPE

PROJECT

CATALOG#

DL340 - 4" ROUND CEILING DOWNLIGHT (IP65) - 1000LM/1500LM

SURFACE CEILING MOUNT



SPECIFICATION

**Application:** This DL Series covered ceiling mount down light can be used in both interior and outdoor settings. Typical locations are in commercial and retail spaces that require lighting from above. Dark Sky compliant.

**Housing:** Constructed from a round one piece high grade aluminum extrusion, die cast aluminum top plate, and faceplate with a powder coat finish. Includes COB LED, reflector, LED driver and a mounting plate. Top plate is secured to housing by threads. Mounting plate is secured to top plate with two (2) flat head phillips stainless steel screws and installs onto an electrical junction box. Consult factory for junction box mounting sizes.

**Mounting:** Fixture includes a mounting bracket for installation directly to a 3-1/2" or 4" octagonal electrical junction-box.

**Faceplate:** Round one piece extruded aluminum with powder coat finish, clear tempered glass lens, and silicone o-ring. Faceplate is secured to the housing by threads.

**Reflector Construction:** One piece, heavy-gauge aluminum reflector prevents ugly dents during shipping and installation. Deeply mounted singular LED provides 50 degree visual cutoff for a glare-free appearance.

**Lumen Maintenance:** Minimum 50,000 hours L70 life based on ANSI TM-21 calculations from LM80 standardized test results. See ordering guide for delivered lumens.

**Dimming:** (-DUN) option is a universal dimming system that works with most 3-Wire ELV, 2-Wire Incandescent and 5-Wire 0-10V fluorescent dimmers.

**Electrical:** AC 50/60Hz Electronic Direct Current Class 2 driver integrally mounted. Power Factor > 0.90. For cold weather applications (-22°C and above) use -DUN driver option.

**Emergency Options:** Emergency LED Battery Back-up available, remotely mounted adjacent to housing by installer. When AC power fails, the device immediately switches to the emergency mode, operating the LEDs for a minimum of 90 minutes. Remote test switch, plate cover and junction box included.

**Caution:** LITON recommends use of surge protectors on the power entering LED Housings. Surge damage is not covered by warranty.

**Warranty:** Covered by a 5 Year Warranty to be free of defects in materials and craftsmanship. Fixture should not be installed in applications with ambient temperature above 60 degrees C. Doing so will result in reduced lamp life and voided warranty.

**Note:** Dark Sky compliant.

**Listing:** ETL / cETL Listed. Suitable for wet location. Assembled in USA. (IP65).

**Finish:** A 7-stage electrostatic, polymer process provides a finish that delivers outstanding durability, superior anti-aging, resistance to corrosion and UV-degradation Available in Black, White, Silver\* and Bronze\*.

**Beam Spread:** Available in Wide Flood, 2° Pencil Beam, Narrow Spot, Spot, Flood and Wall Wash Optic.

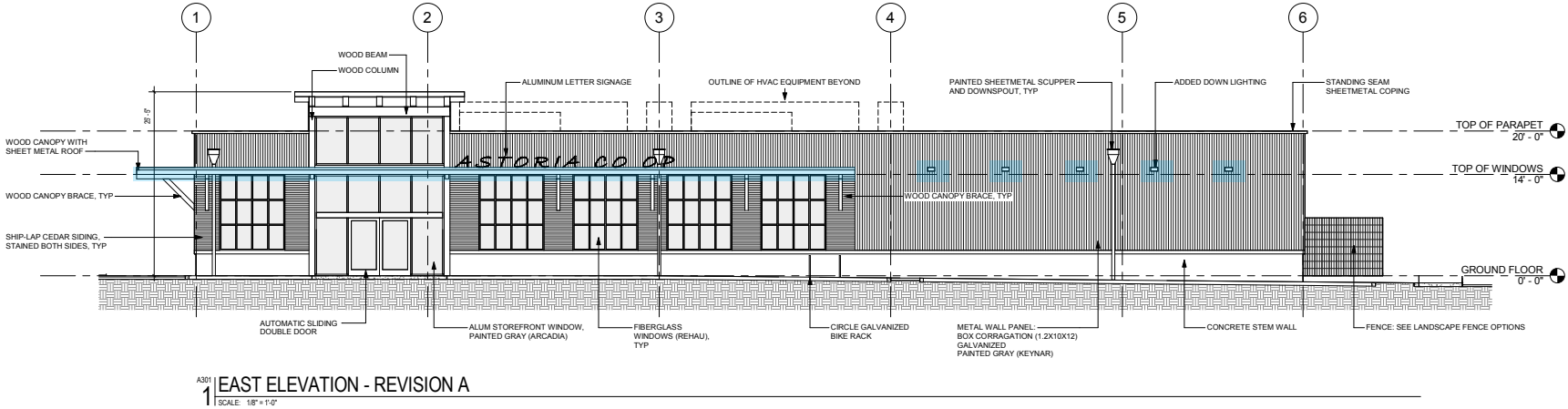
**LED:** Energy efficient Chip-On-Board (COB) Singular LED Light Source provides for smooth uniform light output, eliminating the multiple shadow effect seen by multiple LED Source products. Binned with 4-step MacAdam ellipses as recommended by ANSI Standard. Available in 4000K, 1700K\*, 2500K\*, 2700K, 2700K, 97CRI\*, 3000K, 3000K, 97CRI\*, 3500K, 5000K and 5600K.

- Benefit:**
- Energy efficient, low glare LED Chip-On-Board light engine
  - Beam spreads include NS, SP, FL, WFL and 2° Pencil Beam
  - Emergency back up available
  - Singular COB Light Source
  - 5 year limited warranty
  - Exclusive Wall Wash optics



ORDERING EXAMPLE : DL340B-B02-DIN-T17-EMAC

DL340	FINISH	LUMEN	BEAM SPREAD	DIMMING	LED	OPTIONS
DL340	<b>B</b> :Black	<b>Blank</b> :1000lm (15W)	<b>Blank</b> :Wide Flood	<b>Blank</b> :Non-Dimming (120V)	<b>Blank</b> :4000K	<b>Blank</b> :None
	<b>W</b> :White	<b>-L15</b> :1500lm (22W)	<b>-B02</b> :2° Pencil Beam	<b>UE-DUN</b> :UniDim™ (120V/277V)	<b>-T17</b> :1700K*	<b>-EMAC</b> :Emergency Back Up
	<b>BZ</b> :Bronze		<b>-B10</b> :Narrow Spot		<b>-T25</b> :2500K*	<b>-FR</b> :Frosted Lens
	<b>S</b> :Silver*		<b>-B20</b> :Spot		<b>-T27</b> :2700K	
	<b>CO</b> :Copper		<b>-B45</b> :Flood		<b>-T27-C97</b> :2700K, 97CRI*	
	<b>Penny*</b>		<b>-BWW</b> :Wall Wash Optic		<b>-T30</b> :3000K	
					<b>-T30-C97</b> :3000K, 97CRI*	
					<b>-T35</b> :3500K	
					<b>-T50</b> :5000K	
					<b>-T56</b> :5600K	



PHILIPS  
Stonco

Wall mount

LytePro LED Sconce

LPW16



Project:

Location:

Cat.No:

Type:

Quantity:

Notes:

The Philips Stonco LytePro LED Small Wall Sconce LPW16 features outstanding value in a compact, architectural design. This wall sconce features state-of-the-art, long-life and maintenance savings, in a combined discreet LED package with high precision over-optic design. This powerful and precise combination offers outstanding energy savings with excellent photometric performance. LPW16 is ideal for entryways and corridors in addition to wall lighting applications requiring strong lateral spacing and forward pattern projection.

Stocked luminaires – Ordering guide (LPW16 products are only available in the following stock luminaire configurations shown)

Catalog Number	Description	Master Pack, Qty	UPC Code
LPW16-58BZ	LPW16, 30W, 530mA, 4000K, 120-277V, Bronze textured paint	6	786034960540
LPW16-51BZPCB	LPW16, 30W, 530mA, 4000K, 120V, Bronze textured paint, w/button photocell	6	786034960557
LPW16-78BZ	LPW16, 40W, 700mA, 4000K, 120-277V, Bronze textured paint	6	786034960502
LPW16-78DGY	LPW16, 40W, 700mA, 4000K, 120-277V, Dark gray textured paint	6	786034960489
LPW16-71BZPCB	LPW16, 40W, 700mA, 4000K, 120V, Bronze textured paint, w/button photocell	6	786034960519

Stocked accessories - Ordering guide (Must be ordered separately)

Catalog Number	Description	Master Pack, Qty	UPC Code
LPWCVRPLT-BZ	LPW Universal wall cover mounting plate, Bronze textured paint	(none)	786034960618



P. Other Design Elements Encouraged.

1. Commercial.

- a. Canvas awnings or fixed canopies for rain protection.

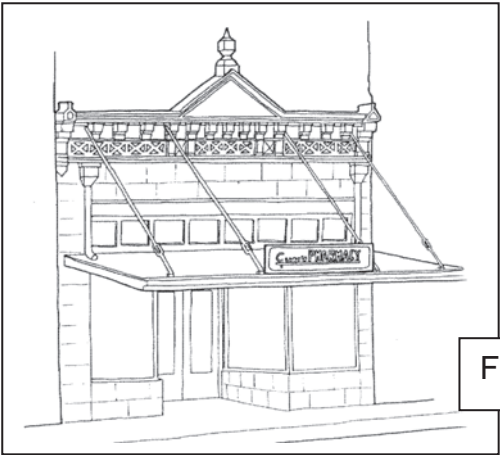
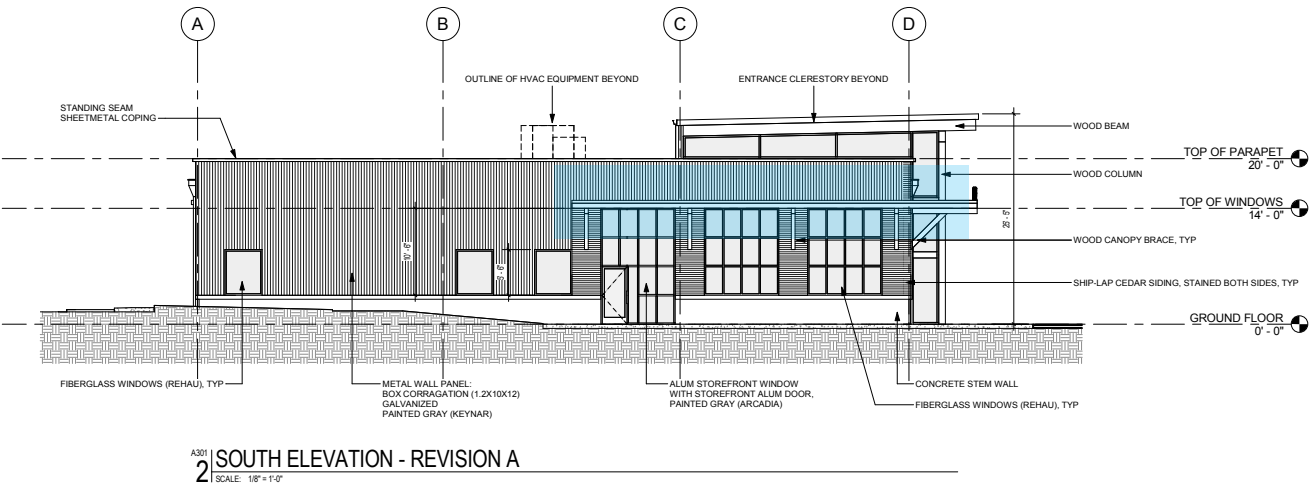
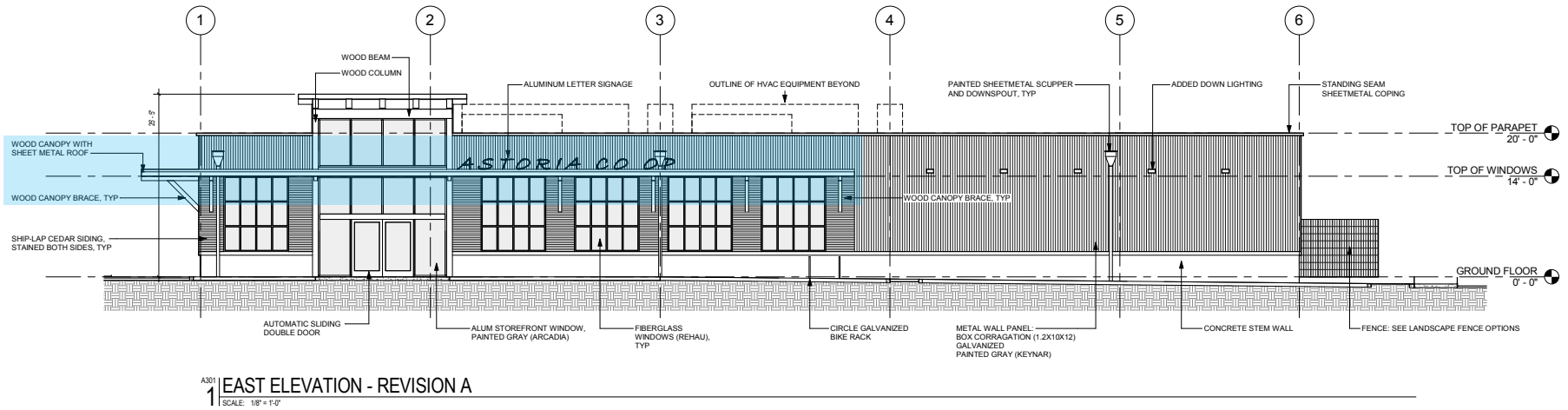
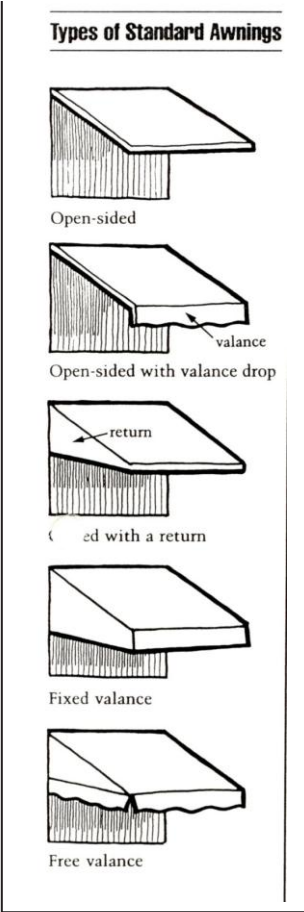


Figure P.1.





14.030. OTHER APPLICABLE USE STANDARDS.

A. Building Orientation.

1. Development projects should form visually continuous, pedestrian-oriented streetfronts with no vehicle use area between building faces and the street.
  - a. Exceptions to this requirement may be allowed to form an outdoor space such as a plaza, courtyard, patio, or garden between a building and a sidewalk. Such a larger front yard area should have landscaping, low walls, fencing, railings, a tree canopy, or other site improvements.
2. New uses should be sited to take advantage of the Columbia River and hillside views.
3. If the proposed project is large or situated so as to become an entrance or major focus of the City, the design should recognize the project's prominence and should be both compatible with its surroundings and complementary to the City as a whole.

B. Building Massing.

1. Buildings should have a floor area ratio on their lots of at least 1:1 (One square foot of building area for one square foot of lot area), in order to maximize use of the land.
2. Buildings should be a minimum of 24 feet in height from grade to highest point of the structure, excluding those features exempt from building height as identified in Development Code Section 3.075.
3. The height, mass, and scale of buildings should be compatible with the site and adjacent buildings. Use of materials should promote harmony with surrounding historic structures and the character of the waterfront.

C. Access and Parking Design.

1. All uses which are served by an alley, local street, or collector street should have alley or street vehicular access and egress. Curb openings onto Marine Drive or Exchange Streets are discouraged. Parking lots should be on the interiors of blocks or behind buildings, and should be designed to be as unobtrusive as possible.
2. Building facades and entries should face the adjacent street. Main entrances should face a connecting walkway with a direct pedestrian connection to the street without requiring pedestrians to walk through parking lots or across driveways.

C. Access and Parking Design.

3. Parking areas should be shared among various uses where a development or block is planned as a whole. On-street parking on internal streets may be counted towards the off-street parking requirements with the approval of the Community Development Director.

D. Landscaping.

1. Street trees should be planted within the right-of-way along both sides of the streets within the Gateway Overlay Zone.
  - a. Spacing should be 30 feet on center, depending on species and branching habit.
  - b. Minimum size of deciduous trees should be 2" caliper, with an upright form.
  - c. Mature branching height should be a minimum of 15'.
  - d. Durable tree grates and trunk protectors should be installed.
2. Areas between trees should be landscaped with a variety of shrubs and perennials, with an emphasis on flowering species.

E. Underground Utilities.

This provision shall apply only to utility lines to be installed for new construction. Utility lines, including, but not limited to, electricity, communications, street lighting and cable television, shall be required to be placed underground. Appurtenances and associated equipment such as surface-mounted transformers, pedestal-mounted terminal boxes and meter cabinets may be placed above the ground, and shall be screened by sight obscuring fences and/or dense landscape buffers. The Design Review Committee may waive the requirements of this section if topographical, soil, or other conditions make such underground installations or screening of above ground equipment unreasonable or impractical. The applicant shall make all necessary arrangements with the serving utility or agency for underground installations provided hereunder; all such installations shall be made in accordance with the tariff provisions of the utility, as prescribed by the State Public Utilities Commissioner.



